

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date : 21 November 2017

Report of

Assistant Director, Planning,
Highways & Transportation

Contact Officer:

Andy Higham
Kevin Tohill
Gary Murphy

Ward:

Southgate

Ref: 17/02342/FUL

Category: Full Application

LOCATION: Ever Ready House, 93 Burleigh Gardens, N14 5AJ

PROPOSAL: Change of use of existing building and part 3rd floor extension to provide 66 bed hotel with associated external alterations, landscaping and car parking.

Applicant Name & Address:

Palmers Green Investments Limited
c/o Agent

Agent Name & Address:

William Kumar
Five Development Consultancy LLP
43 Athenaeum Road
Whetstone

RECOMMENDATION:

It is therefore recommended that planning permission be **GRANTED** subject to conditions.

1. Site and Surroundings

- 1.1. The application site comprises a vacant part 3 and 4-storey former office building with car park to the rear. The existing 4th storey takes the form of a mansard roof. The building is situated immediately outside the boundaries of Southgate Town Centre.
- 1.2. The surrounding area comprises Southgate Town Centre with a mix of retail, commercial and office uses to the east of the site. Additionally, there are residential properties sited immediately to the west on Burleigh Gardens and north-west on Crown Lane. These are within the London Borough of Barnet; the borough boundary runs along the western edge of the site. Directly opposite the application site is a four-storey building with commercial use at ground floor and residential above.
- 1.3. The site is located outside of the Southgate Circus Conservation Area, and does not relate to a Listed Building.

2. Proposal

- 2.1. The applicant seeks full planning permission for the change of use of existing building and part 3rd floor extension at the rear to provide a 66-bed hotel (Use Class C1) with associated external alterations, landscaping and car parking.
- 2.2. The proposal involves the change of use of this former officer building, which is currently vacant. It has most recently been used for education purposes following a grant of planning permission for this D1 use in 2011. A modest roof extension is being proposed to the rear at third floor level with additional floorspace of 211sqm on top of the existing 1984sqm.
- 2.3. The proposed roof extension will be finished in a mixture of rainscreen cladding and render. The rest of the existing building is also to benefit from an upgrade, it is proposed to change the external appearance by modernising the fenestration using a palette of render, new brick slip cladding system, metal cladding, new projecting window openings and projecting canopy to the hotel entrance.
- 2.4. The existing building benefits from a rear car park and vehicle access at the eastern end. As part of the proposal to support the hotel use this will continue to be used as a car park, and for servicing. This will be re-surfaced to provide an improvement, with landscaping features and sustainable drainage measures put in. The car park will provide 33 parking spaces, including 10 disabled spaces, and the car park will continue to be accessed from Burleigh Gardens.

3. Consultations

3.1. *Neighbours*

Letters were sent to 129 adjoining and nearby residents on 12.06.17 (including addresses within the London Borough of Barnet which adjoins the site). To date two objections have been received from addresses on Burleigh Gardens which raised the following planning considerations:

- *The site is more suited to residential use and there is no demand for a hotel use in this location.*
- *Hotel use will result in increased traffic from staff, guests and servicing vehicles. This will impact negatively on road conditions within Burleigh Gardens.*
- *Proposed hotel use is too close to existing residential properties and will adversely affect residential amenity.*

3.2. *Internal*

Traffic and Transportation - No objections subject to conditions.

Designing out Crime - No comment.

Economic Development - No comment.

Environmental Health - No objections subject to conditions relating to acoustic details.

SuDS - The SuDS strategy is considered to be broadly acceptable, the applicant should however justify the intended use of an underground storage tank over other feasible above ground features.

Regeneration, Leisure and Culture - No comment.

3.3. *External*

London Fire and Emergency Planning - No comment.

Thames Water - No objections subject to a condition dealing with drainage details.

London Borough of Barnet - No comment.

- 3.4. Site notice displayed on 13.07.17
Press notice advertised on 21.06.17

4. **Relevant Planning History**

- 4.1. TP/10/0583 - Planning permission **granted** for the change of use from B1 to D1 use to provide an adult education facility. This use was implemented without discharging of conditions.
- 4.2. P14-00048PLA - Planning permission **refused** for the conversion of the existing college building into 26 self-contained flats (comprising 13 x 1-bed, 10 x 2-bed and 3 x 3-bed) including the construction of a part fourth floor with mansard roof, reconfiguration of the car park and an external fire escape staircase at the side. **Appeal submitted and dismissed.**
- 4.3. 14/02947/FUL - Application **withdrawn** for conversion of existing building into 25 self-contained flats (comprising 11 x 1-bed, 10 x 2-bed and 4 x 3-bed) involving construction of a part fourth floor within a mansard roof, reconfiguration of car park at rear and replacement external fire escape at rear.

- 4.4. 15/03227/PRJ - Prior Approval required and **refused** for change of use from Office (Use Class B1 (a)) to residential (Use Class C3) 22 self-contained flats (comprising 1 x 3-bed, 7 x2-bed, 14 x1-bed). **Appeal submitted and dismissed.**
- 4.5. 15/01946/PREAPP - Proposed demolition of part of existing car park and erection of a part 6, part 7-storey hotel (C1) with restaurant (A3) and associated works - Pre-application advice given

5. Relevant Policy

- 5.1. The Development Management Document (DMD) policies have been prepared under the NPPF regime to be NPPF compliant. The DMD provides detailed criteria and standard based policies by which planning applications will be determined.
- 5.2. The policies listed below are considered to be consistent with the NPPF and therefore it is considered that due weight should be given to them in assessing the development the subject of this application.

5.3. London Plan (2016)

- 2.2 London and the wider Metropolitan area
- 2.15 Town centres
- 3.9 Mixed and balanced communities
- 4.1 Developing London's economy
- 4.2 Offices
- 4.5 London's visitor infrastructure
- 4.6 Arts, culture, sport and entertainment provision
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.10 Urban greening
- 5.11 Green roofs and development site environs
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.16 Waste self sufficiency
- 6.3 Assessing the effects of development on transport capacity
- 6.8 Coaches
- 6.9 Cycling
- 6.10 Walking
- 6.12 Road network capacity
- 6.13 Parking
- 7.1 Building London's neighbours and communities
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.5 Public realm
- 7.6 Architecture
- 7.8 Heritage assets and archaeology
- 7.15 Reducing noise and enhancing soundscape

5.4. Core Strategy

CP9 Supporting community cohesion
 CP11 Recreation, leisure, culture and arts
 CP12 Visitors and Tourism
 CP16 Taking part in economic success and improving skills
 CP17 Town Centres
 CP19 Offices
 CP20 Sustainable energy use and energy infrastructure
 CP21 Delivering sustainable water supply, drainage and sewerage infrastructure
 CP22 Delivering sustainable waste management
 CP24 The road network
 CP26 Public transport
 CP25 Pedestrians and cyclists
 CP30 Maintaining and improving the quality of the built and open environment
 CP31 Built and Landscape Heritage
 CP32: Pollution
 CP46 Infrastructure Contribution

5.5. Development Management Document

DMD10 Distancing
 DMD17 Protection of Community Facilities
 DMD 22 Loss of Employment Outside of Designated Areas
 DMD27 Southgate District Centre
 DMD31 Development Involving Tourism and Visitor Accommodation
 DMD37 Achieving High Quality and Design-Led Development
 DMD44 Conserving and Enhancing Heritage Assets
 DMD45 Parking Standards and Layout
 DMD47 New Roads, Access and Servicing
 DMD48 Transport Assessments
 DMD49 Sustainable Design and Construction Statements
 DMD50 Environmental Assessment Methods
 DMD51 Energy Efficiency Standards
 DMD64 Pollution Control and Assessment
 DMD68 Noise
 DMD69 Light Pollution
 DMD70 Water Quality

5.6. Other relevant policy/guidance

National Planning Policy Framework
 National Planning Practice Guidance

6. **Analysis**

Principle of Development

- 6.1 Policies CP17 of the Core Strategy and DMD27 of the Development Management Document relate to Town Centres. Additionally, policies CP12 of the Core Strategy and DMD31 relate to visitors, tourism and visitor accommodation.

- 6.2. The Mayor of London has identified a potential growth of 40,000 net additional hotel bedrooms by 2036, a need to reduce pressure on central London, and the need to provide more affordable hotel capacity (London Plan policy 4.5). Council planning policy supports proposals for a wide range of visitor accommodation, including hotels. Such accommodation should primarily be directed towards Enfield's town centres, including Southgate and other locations with good public transport accessibility. This would help to support the enhancement of Enfield's visitor and tourism potential.
- 6.3. The provision of a 66-bed hotel on this site, with good access to public transport will help to address a shortage of hotel rooms in the borough, as acknowledged by Core Policy 12. Furthermore, it will help contribute towards the strategic London Plan target to achieve 40,000 net additional hotel rooms in London by 2036.
- 6.4. The proposed hotel site is situated just outside of the designated Southgate town centre boundary, though there are no physical or perceptual barriers to the site that demarcate this from the town centre. Effectively this site functions as part of the wider Southgate town centre and it is not until you go immediately to the west of the site that the character of Burleigh Gardens changes to residential.
- 6.5. Notwithstanding the above the site is outside of the town centre boundary so must be assessed accordingly. To that end it is recognised that the site is located in close proximity to Southgate tube station on the Piccadilly line. The tube station is approximately 90 metres northeast of the site (2 minute walk) which has very good accessibility to central London. The PTAL rating for the site is PTAL 4, which corresponds to a 'very good' level of accessibility to public transport networks. Core Policy 12 and DMD 31 both state that if new visitor accommodation cannot be located in town centres then it should be located in locations with "good transport accessibility" to central London. As this site benefits from PTAL level 4, with very good access into Central London, and is immediately adjacent to the existing town centre is considered to be a suitable location for a hotel in accordance with Core Policy 12 and DMD31.
- 6.6. Policy DMD31 also seeks to ensure that development involving new hotels are appropriate in terms of their size, have an acceptable impact on the character of an area, do not lead to an over concentration in a locality, do not adversely affect surrounding residential amenity and that there are no adverse impacts on highway safety. A range of criteria (a to h) upon which to assess the suitability of a location for a hotel use are set out in DMD31 and repeated below.
 - (a) The size and character of the site or building are suitable for the proposed use;
 - (b) The proposed use will be compatible with the character and appearance of the area;
 - (c) The proposal does not result in an over concentration of hotel, boarding and/or guest houses in that locality;
 - (d) The residential amenities of local residents will not be adversely affected by way of unacceptable increases to traffic and parking in the area;
 - (e) The existing environment or transport system will not be adversely affected by way of unacceptable increases to traffic and parking in the area;

- (f) The proposal has adequate servicing arrangements and provides the necessary off-highway pickup and set down points for taxis and coaches;
 - (g) The proposal provides on-site accommodation and training for staff, where the scale of development allows; and
 - (h) At least 10% of all hotel rooms will be provided to wheelchair accessible standards.
- 6.7. In consideration of the current proposals against the above criteria the appeal decision against the refusal of application ref: P14-00048PLA, for residential use on the site is considered to be material. The appeal decision for this residential scheme found that the proposed use would not result in any undue impact on neighbouring residential amenity. Given the similarities of operation between residential flats and use of a hotel, on balance it is considered that there would be no undue impact on neighbouring residential amenity as a result of this change of use.
- 6.8. The site also has the ability to provide off street parking and has adequate space for servicing arrangements on site. Provision is made for 10% of rooms to be wheelchair accessible, in line with DMD31 and London Plan policy 4.5.
- 6.9. The building has a history of B1(a) office and education use so the loss of both uses is considered. The most recent use of the building has been for education purposes following the grant of planning permission TP/10/0583. It was later established through appeal decision APP/Q5300/W/15/3134700 (dated 19 Feb 2016) in relation to a Prior Approval application (15/03227/PRJ) that the building was used as an education facility between 2011 and 2014, and this application confirms also that the college use closed in June 2015. It is also confirmed that since the college used ceased that part of the ground floor has been used as office by the applicant. For the purposes of this application the proposal is assessed on the basis that it would result in the loss of an education use, with that being the most recent permitted use of the building.
- 6.10. The loss of office floorspace in this location was accepted in principle, as evidenced by the grant of planning permission TP/10/0583 for education use. The building has therefore not contributed to the supply of B1(a) office floorspace for approximately 6 years. As such there is no objection raised to a hotel use on these grounds. Furthermore, the building has been marketed unsuccessfully for B1(a) office use since the college vacated in 2015, and for a considerable period of time prior to it being used as a college, demonstrating a clear lack of demand for office floorspace in this location with multiple office buildings being available for let in Southgate. The marketing initiatives took the form of marketing boards, marketing brochure distribution and on-line advertising.
- 6.11. The previous use as D1 education has also been marketed since the college vacated in 2015, but with no interest shown from similar operators. The refused appeal decision for the creation of residential flats (P14-00048PLA) in 2015 saw no issue with the loss of education use and the officer report for the application accepted that the building had been marketed extensively and that this showed there was a lack of demand from D1 tenants for the space.
- 6.12. It has been demonstrated that despite marketing attempts there is a lack of demand for office and/or education use of this building. This lack of demand can be attributed to the floor plates, general condition of the building not being

suitable for modern office requirements, limited floor to ceiling heights, dated mechanical and electrical engineering provision and availability of office floorspace for let in the Southgate area. The building owner has as a result of the marketing exercise had expressions of interest from a number of hotel operators, which demonstrates a healthy demand for additional hotel accommodation in this location which is why the change of use application is being pursued.

- 6.13. In view of the above decision, and the fact that officers have previously accepted the marketing efforts as demonstrating a lack of demand for B1(a)/D1 occupiers in this location there is no objection to the principle of a hotel use of the building over office or education. The principle of a hotel at this location is therefore considered acceptable under Core Policy 12 and Development Management Policy DMD31.

Character and Appearance

- 6.14. Policy DMD37 of the DMD encourages achieving a high quality and design led development that should be suitable for its function and appropriate in its context with appropriate regard to its surroundings. Additionally, policy 7.4 of the London Plan specifies the need to respect the character of the surrounding area but also make a positive contribution to the places identity. This policy is re-iterated by CP30 of the Core Strategy as well as the fundamental aims of the NPPF.
- 6.15. The existing building is of little architectural merit, has a neutral impact on the character of the area and there are no redeeming features for retention. The proposed design for the roof extension is in keeping with the character and appearance of the proposals to refurbish the building. The materials palette will ensure the rearward roof addition ties in with the rest of the building façade works. The proposed new materials palette and articulation to the building will improve the overall appearance and make this more interesting visually within the streetscene and the use of render is in keeping with the residential properties to the west. Further visual interest and articulation is to be achieved by incorporating projecting windows, these will help to give the building a vertical emphasis and depth to the street facing façade which would be an improvement over the existing flat elevation, and the new entrance canopy will help make this access point more legible. These works will create a modern looking building that would integrate well with the surrounding area, and to ensure the quality of the final build a condition is recommended requiring the submission and approval of all external materials.
- 6.16. Officers consider that the proposed external works would result in this building making a positive architectural contribution to this locality.

Scale and Massing

- 6.17. The extension is proposed to the rear of the building, at third floor, which is effectively enlarging the existing 4th storey. Currently the 4th storey comprises of the mansard roof element, and the proposal is to extend to the rear of this with a new, more contemporary flat roof addition. This would extend over the building footprint of the existing 3-storey rear projection. The massing and bulk of this would blend in with the existing built form.

- 6.18. It is worth noting that the appeal decision against the refusal of application ref: P14-00048PLA has previously given consideration to scale and massing of the roof extension in terms of how this impacts on neighbour amenity, and this sets the parameters for future development. The appeal decision did not find that the extension to the rear would result in conditions harmful to neighbouring residential amenity in terms of its size and the Council's previous objection to this element was only on the grounds of unacceptable overlooking from new windows due to their proximity to the site boundary. Whilst the proposed extension is marginally larger on the western side than previously due the removal of a set in it is not considered to be significantly larger, such that a different conclusion should be reached this time. How this scheme responds to the issue of overlooking and loss of privacy is discussed in the following section.

Impact on Neighbouring Residential Amenity

- 6.19. The nearest residential properties are those immediately to the west and south-west on Burleigh Gardens, and to the north-west on Crown Lane. Immediately opposite the building are existing flats also.
- 6.20. As referred to above, no objection was raised previously in either the Council's refusal or the dismissed appeal, to the scale and massing of the roof extension on the grounds that this would unduly harm neighbour amenity. This was however objected to by the Council on the grounds that that the proposed residential use and the additional fenestration proposed to the western elevation required to support residential use would result in a poor relationship with neighbouring residential properties through increased overlooking and loss of privacy above ground floor level.
- 6.21. At appeal, the Inspector took the view that the relationship between the application building and 91 Burleigh Gardens would only afford oblique views into the rear garden from upper floor windows, and whilst the proposed residential use of the building may result in a greater intensity in the use of these rooms throughout the day, this would not result in material harm in terms of overlooking or loss of privacy to existing and future occupiers of this neighbouring property.
- 6.22. Although the Council's concerns to do with overlooking of neighbouring residential properties were not supported in the aforementioned appeal decision, this application has sought to address this relationship in any event by proposing to make alterations to the window arrangement on the western elevation (facing 91 Burleigh Gardens).
- 6.23. It is proposed to remove a number of windows and reduce the size of existing openings facing west, as a result of internal layout changes to suit the hotel use. Furthermore proposed new openings above ground floor have been designed as projecting angled windows which will only afford outlook from first, second and third floor hotel rooms to the north rather than north-west. This equates to a 45 degree orientation which will prevent direct views towards the rear of neighbouring residential properties on Burleigh Gardens. This additional mitigation is considered to be an appropriate design response and is welcomed.
- 6.24. No objection is raised on the grounds of noise and disturbance to residential properties from the proposed hotel use. This is an existing commercial

building on the very edge of Southgate town centre, and although vacant could be utilised for B1(a) office or D1 education. Both uses would potentially attract large numbers of staff and visitors throughout the day. Similarly, a hotel use would also potentially attract large numbers of staff and visitors, but typically this would be greatest at weekends and there would be more of an even flow of visitors arriving and leaving throughout the day in contrast to an office or education use which would have more defined peak periods that could impact on neighbouring amenity. It is also relevant that the building is detached which will help to reduce the potential for noise and disturbance issues for the closest residential neighbours, additionally there is no bar area proposed within the hotel and the main hotel entrance point will be towards the eastern end of the building well away from neighbouring residential dwellings. Vehicle movement activity will be to the rear of the building, which is no different to existing parking and servicing arrangements. Furthermore, the external fire escape staircase that it is currently situated on the western side of the building is to be replaced, broadly in the same position and the replacement staircase will be enclosed which will reduce the potential for overlooking of neighbouring properties when in use.

- 6.25. On balance, it is considered that the use of the building as a hotel would not unduly harm the amenity of neighbouring residential dwellings as a result of additional noise and disturbance, subject to any approval securing through condition the submission and approval of further details of noise levels from any plant that is to be installed. The surrounding area has a range of different town centre uses, and there is an existing night time economy associated with Southgate town centre. The introduction of a hotel to this location would therefore be appropriate.
- 6.26. In summary, it is considered that the proposed development would not be unduly harmful to the amenity of nearby residential occupiers, through noise and disturbance, overlooking and loss of privacy, having regard to policies DMD31, 34 and DMD68 of the Development Management Document.

Traffic and Transportation

- 6.27. The site has a PTAL of 4, which indicates very good access to public transport services.
- 6.28. The subject site is located in the Southgate CPZ, which is operational from Monday to Saturday between 8am-6:30pm. There is also the Southgate (one hour) CPZ which is operational between Monday to Friday from 11am-12noon.

Parking:

- 6.29. The proposals involve the retention of the existing car park and the provision of 33 off-street spaces (including more than 10% disabled provision). Furthermore, in accordance with London Plan policy, 10% active and passive electric vehicle parking spaces have been proposed and these will be secured through condition.
- 6.30. The applicant has undertaken analysis of likely trip generation based on two similar size and types of hotel using the TRICS database and this demonstrates that the number of trips associated with a 66-bed hotel is not higher than the currently consented use, in fact it shows an anticipated

reduction. It is also likely that the distribution by time will shift from the AM and PM peaks to the periods before check-out and before check-in for hotel customers. There are no concerns therefore from a transportation perspective relating to impact on the surrounding road network.

- 6.31. Access to the car park will be via the existing access point, which is acceptable, and this can accommodate servicing vehicles (see below). A new barrier controlled entrance and exit system will be installed, and set back from the highway to allow space for vehicles to stop clear of the highway.

Cycle Parking:

- 6.32. A total of 3 three long stay and two short stay cycle parking spaces are proposed, in line with London Plan minimum requirements. It has been demonstrated that these will be in a covered, convenient, secure and accessible location within the car park. The provision of these will be secured by condition.

Access:

- 6.33. As stated above vehicular access will continue to make use of the established access point.
- 6.34. Pedestrian access will be from Burleigh Gardens, which is the same as existing arrangements. This enables step free access and is acceptable.

Access, Delivery and Servicing Arrangements:

- 6.35. The use of the existing access route is proposed and an issue was raised relating to circulation within the site and the ability of larger vehicles to safely and conveniently access the site. As requested the applicant has provided further information setting out the largest vehicle that will need to access the rear of the site, because of the small scale nature of the hotel, is a 7.5t box van. Swept path tracking diagrams demonstrate that this size of vehicle can safely enter, exit and turn and this is accepted by Transportation. It has been explained that the number of deliveries daily is expected to be three to four, and these would take place between 07.00 and 10.30am typically. This is not expected to have an adverse impact on the surrounding highway network.
- 6.36. For hotel uses the London Plan requires one coach parking space for every 50 hotel rooms. The applicant has been asked to further clarify their position given the lack of any dedicated on-site coach lay-by.
- 6.37. In response, it has been set out that the constraints of the site and the need to maintain the existing vehicle access it would be impractical to provide on-site coach parking. As set out in the Transport Statement, and evidenced by the expressions of interest from hotel operators it is intended that this would be operated as a budget range hotel. Typically, this would cater for single business users and short stay tourists.
- 6.38. The business model and operational/marketing approach typically adopted by the budget range of hotels will seek to exclude coach parties at booking stage. Furthermore, no incentive will be offered to coach operators, further decreasing the attractiveness of the site for large group bookings. Arrivals by

coach are therefore not anticipated and will be declined. For this reason, no dedicated provision close to the site for coaches has been proposed, particularly as Burleigh Gardens is relatively narrow.

- 6.39. In the unlikely event a coach visit was required they could set down temporarily for pick-up and drop-off making use of existing on-street restrictions which allow for such use on Chaseside (A111). Further details of the on-site management arrangements that would be put into practice to discourage coaches will need to be set out in a Coach Management Plan.
- 6.40. Any consent would be subject to a condition requiring the submission and approval of a Coach Management Plan, included within this will need to be measures that will be employed by the hotel operator to discourage coach party bookings, and arrangements that will be put into practice in the event that coaches do drop-off and/or pick up, as well as taxi's.
- 6.41. Furthermore, a detailed Delivery and Servicing Management Plan will be required, included within this will need to be arrangements of delivery booking systems and for the collection of refuse. The applicant has indicated this will be by way of a 'just in time' operation, which is the same as existing that takes place from Burleigh Gardens. This means that commercial waste is collected by a private waste contractor and given that hotel occupation levels change (so to would waste generated) the waste will be collected as required in order to minimise collection costs (this is referred to as a 'just in time' arrangement). On site storage would be used in between collection days. It would not make economic sense for commercial waste collection to take place daily and the 'just in time' arrangement is typically employed on commercial uses such as hotels. Further details of the designated refuse enclosure will also be secured through condition.

Construction Vehicle Management:

- 6.42. The nature of the proposal means the development does not require the provision of a separate Construction Traffic Management Plan, notwithstanding this the Construction Methodology condition will require details relating to construction hours and vehicles.

Energy and Sustainability

- 6.43. Policies 5.2 & 5.3 of the London Plan and Policy 20 of the Core Strategy recognise that not all developments are capable of achieving significant improvements over building regulations, and makes provision to mitigate for any shortfall through agreed allowable solutions. As this is predominantly a change of use and refurbishment of an existing building then it is recognised that there are very real practical challenges involved when it comes to retrofitting and fully meeting sustainable requirements.
- 6.44. London Plan policies 5.3 and 7.2 of the adopted London Plan seek to mitigate climate change by reducing carbon dioxide emissions. Developments are required to make the fullest contribution to tackling climate change by minimising carbon dioxide emissions in accordance with the energy hierarchy; Be Lean: Be Clean: Be Green. The energy strategy is targeting carbon dioxide emissions (9%) through energy efficiency measures and improvements to the building fabric. It also proposes the installation of solar thermal/ PV panels at roof level and the incorporation of new green roofs. It

does not appear that the use of solar thermal/PV panels have been accounted for in the energy strategy and this needs to be rectified. Further detail should be provided in the form of a revised Energy Strategy to demonstrate how the change of use and fit-out would comply with DMD51 on carbon dioxide emissions. This can be secured by condition, which the applicant is agreeable to.

- 6.45. A BREAAAM Pre-Assessment Report has not been submitted as part of the application and this will be secured by condition. The applicant has indicated that BREEAM rating "Very Good" is to be met, however in accordance with DMD50 they should be targeting "Excellent" rating. The applicant has confirmed that they would be agreeable to a condition requiring them to target a BREEAM rating that exceeds "Very Good" and where this is not possible it will need to be adequately demonstrated. This is considered to be a pragmatic approach.
- 6.46. Finally, the applicant will need to submit a water efficiency report demonstrates that efficiency measures can be achieved in accordance with DMD policy 58. This can be secured by condition.

Noise

- 6.47. Potential noise impacts associated with the use are a material consideration, particularly as there are residential neighbours immediately to the west, and north-west of the site, as well as opposite.
- 6.48. London Plan policy 7.5 aims to reduce noise and enhance soundscapes. Measures to be taken here include separating noise sensitive development from major noise sources and supporting technologies and practices aimed at reducing noise at the source.
- 6.49. DMD 68 states that developments that generate or would be exposed to an unacceptable level of noise will not be permitted. It states that developments must be sensitively designed, managed and operated to reduce exposure to noise and noise generation.
- 6.50. Furthermore, DMD 31 part d) states the following in respects of Hotel Development;

"The residential amenities of local residents will not be adversely affected through noise, disturbance, loss of light or privacy."

- 6.51. An environmental Noise Impact survey was undertaken by KP Acoustics which has enabled criteria to be set for the proposed plant installation to minimise any impact on nearby amenity. Final calculations will be undertaken once the proposed plant kit has been finalised, to which the applicant will accept a condition requiring further information to be submitted. Environmental Health officers have accepted this approach on the basis that a condition is secured requiring the submission and approval of further details. Such details shall set out the sound level generated from all noise generating plant and equipment and state the noise control measures to be employed to ensure the noise from the combined plant does not exceed a level of 10dB(A) below typical background noise levels, at the façade of the nearest residential/noise sensitive property.

Sustainable Drainage

- 6.52. DMD policy 61 states that all developments must maximise the use of and, where possible, retrofit Sustainable Drainage Systems (SuDS). Any proposed SuDS measures should be appropriate for the site conditions, seek to achieve greenfield run off rates as well as maximise the use of SuDS.
- 6.53. As required a Sustainable Drainage Strategy has been submitted by BWB, and sets out the approach to managing surface water on site. The strategy proposes the following; -
- A discharge rate of 5l per second for the total restricted run off rate for the site, which is accepted by SuDS officers;
 - In accordance with the Drainage Hierarchy infiltration has been considered but deemed impractical due to existing ground conditions on site, there are no existing watercourses within the vicinity of the site that would be deemed appropriate to discharge into. It is proposed to make use of a new connection to the surface water sewer;
 - Use of a permeable paving system for the re-surfaced car park;
 - Installation of a pipe network around the building and wider site to catch water prior to discharge;
 - Incorporation of an underground storage tank, this will provide storage for a 100 year plus, 40% flood event;
 - Use of a green roof over the extension, and
 - Potential for incorporating smaller rain gardens/planters to provide treatment for the roof runoff (feasibility to be explored and secured by condition).
- 6.54. Discussions have been had with the applicant to agree changes to the drainage strategy, for example by incorporating the proposed landscaping features as part of the SuDS solution and any other practical above ground measures rather than relying on the underground solution.
- 6.55. The applicant has confirmed that whilst no detailed geological site investigation has been carried out that it is known the site is overlaying a bedrock comprising of London Clay Formation which has very low impermeability, the site is also described as being in a built up urban area and therefore there is limited space from the existing building and surrounding properties for infiltration. For these reasons, the applicants consultant has not recommended the use of borehole soakaways as an alternative. Above ground drainage options have been discounted by the applicant on the grounds of there not being sufficient space around the building and car park for features such as ponds and detention basins. The applicant is open to the suggestion of incorporating smaller rain gardens/planters to provide treatment for the roof runoff, and will look into incorporating these where feasible. A condition is recommended requiring the submission and approval of a revised drainage strategy following more detailed investigations of ground conditions on site and exploration of feasible above ground solutions.
- 6.56. Thames Water have requested that further details of any on/off site drainage are approved through condition, prior to the commencement of works.

- 6.57. Traffic and Transportation have not sought any contributions to mitigate the impacts associated with the proposal. The use of the building as a hotel compared to a fully occupied D1 use, or office is not likely to result in a form of use that requires mitigation by way of s106 contributions.

Community Infrastructure Levy (CIL)

- 6.58 As of the April 2010, new legislation in the form of CIL Regulations 2010 (as amended) came into force which would allow 'charging authorities' in England and Wales to apportion a levy on net additional floorspace for certain types of qualifying development to enable the funding of a wide range of infrastructure that is needed as a result of development.
- 6.59. The development would be liable to a Community Infrastructure Levy contribution as the development involves new commercial floorspace. As the building has been used for its lawful use for 6 months of the previous 36 months then only the additional gross internal area is liable. This equates to the additional 211sqm.
- 6.60. This would result in a CIL contribution of £4220 (211 sq.m x £20, subject to indexation). The Council's CIL charging schedule has a nil charge for hotel development.

7. Conclusion

- 7.1. The proposed hotel use is appropriately designed and would integrate satisfactorily to the surrounding area on this edge of centre location. As discussed above it is considered that the proposal would not be detrimental to residential amenities, or highway safety, having regard to adopted local, regional and national level policies and would make efficient use of this long-term vacant and underutilised site in a sustainable location.

8. Recommendation

- 8.1. It is therefore recommended that planning permission be granted subject to the following attached conditions:

Conditions

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

04001 revP1 Elevations as Existing;
04002 revP1 Elevations as Existing;
01001 revP1 Existing site plan;
02001 revP1 Ground floor existing;
02001 revP1 First floor existing;

02001 revP1 Second floor existing;
02001 revP1 Third floor existing;

02002 revP2 Ground floor proposed;
02002 RevP2 First floor proposed;
02002 RevP2 Second floor proposed;
02002 RevP2 Third floor proposed;
27001 RevP2 Proposed roof plan;

01002 RevP3 Proposed site plan;
90201 RevP1 Landscaping Plan;

External Canopy Study;
Design & Access Statement (May 2017);
BWB Sustainable Drainage Statement BLG-BWB-XX-XX-RD-C-0001; and

00001 RevP1 Block plan

Reason: For the avoidance of doubt and in the interests of proper planning.

3. That development shall not commence until a Construction Methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:

- a. arrangements for wheel cleaning;
- b. arrangements for the storage of materials;
- c. hours of work;
- d. arrangements for the securing of the site during construction;
- e. the arrangement for the parking of contractors' vehicles clear of the highway;
- f. the siting and design of any ancillary structures;
- g. enclosure hoarding details;
- h. A construction management plan written in accordance with the 'London Best Practice Guidance: The control of dust and emission from construction and demolition'.

The development shall be carried out fully in accordance with the approved Construction Methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

4. Prior to commencement of any external building works, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows, cladding and front entrances within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

5. The external landscape works shall not commence until details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas, road markings and all other hard surfacing has been submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out fully in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

6. Prior to the commencement of any external building and/or landscaping works details of all planting and other soft landscaping (including green roofs specification) on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme and green roofs shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

7. The use hereby approved shall not commence until further details of the means of enclosure for the refuse storage facilities have been submitted to and approved in writing by the Local Planning Authority. Facilities for the recycling of waste are to be provided within the development, in accordance with the London Borough of Enfield Waste and Recycling Planning Storage Guidance ENV 08/162. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

8. The use hereby approved shall not commence until details of the siting, number and design of secure/covered cycle parking spaces have been submitted to and approved in writing by the Local Planning Authority. The approved details shall thereafter be installed and permanently retained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

9. The development shall not commence until a revised 'Energy Statement' has been submitted and approved in writing by the Local Planning Authority. Submitted details will demonstrate the energy efficiency of the development shall provide for an improvement in total CO2 emissions arising from the operation of a development and its services over Part L of Building Regs 2010, in accordance with the requirements of Development Management

Policy DMD51. The Energy Statement should outline how the reductions are achieved through the use of Fabric Energy Efficiency performance, energy efficient fittings, and the use of renewable technologies. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, DMD51 of the Development Management Document, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

10. The development hereby approved shall not commence until a BREEAM pre-assessment has been submitted to and approved in writing by the Local Planning Authority. This shall demonstrate how the development will be built to a minimum standard of BREEAM 'Very Good' and how it shall use reasonable endeavours to achieve an 'Excellent'. Rating. Where a rating of 'Excellent' cannot be achieved the applicant shall demonstrate why this is not technically feasible or economically viable.

Prior to the occupation of the approved building, a copy of the Post Construction Certificate verifying that a minimum BREEAM 'Very Good' has been achieved shall be submitted to the local planning authority.

The evidence required shall be provided in the following formats and at the following times:

- a. A design stage assessment, conducted by an accredited Assessor and supported by the relevant BRE interim certificate, shall be submitted at pre construction stage prior to the commencement of superstructure work on site
- b. A post construction assessment, conducted by an accredited Assessor and supported by relevant BRE accreditation certificate, shall be submitted following the practical completion of the development and prior to first occupation.

The development shall be carried out strictly in accordance with the details approved, shall be maintained as such thereafter and no change shall take place without the prior approval of the LPA.

Reason: In the interests of addressing climate change and to secure sustainable development in accordance with the strategic objectives of the council and Policies 5.2, 5.3, 5.7, 5.9, 5.12, 5.13, 5.15, 5.16 of the London Plan as well as the NPPF.

11. Prior to occupation details of the internal consumption of potable water shall be submitted to and approved in writing by the Local Planning Authority. Submitted details will demonstrate reduced water consumption through the use of water efficient fittings, appliances and recycling systems to show consumption will exceed a 25% improvement in water efficiency over notional baseline. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To promote water conservation and efficiency measures in all new developments and where possible in the retrofitting of existing stock in

accordance with Policy CP21 of the Core Strategy, DMD58 of the Development Management Document and Policy 5.15 of the London Plan.

12. Prior to the commencement of the use hereby approved a detailed Servicing and Delivery Management Plan for the management of deliveries and servicing to site has been submitted to and approved in writing by the Local Planning Authority. The strategy shall include hours of deliveries, measures to avoid localised congestion and parking on footways and damage to buildings caused by vehicles. The applicant shall detail a booking system to be operated to co-ordinate the arrival of deliveries to ensure that all associated vehicles can be accommodated within the site with no need to wait on the adjoining highway and also set out a robust enforcement regime to ensure that no unauthorised use occurs. Deliveries and servicing shall thereafter be carried out solely in accordance with the approved details.

Reason: To avoid hazard and obstruction being caused to users of the public highway.

13. Prior to the commencement of the use hereby consented details of a Coach Management Strategy shall be submitted to and approved in writing by the Local Planning Authority. This should set out measures to be employed to discourage coach party bookings, an appropriate means of managing coaches in the event that they do arrive to site, and identify appropriate management arrangements for the drop-off and pickup of coach passengers in such an event. The use shall operate fully in accordance with the approved Plan thereafter.

Reason: To avoid hazard and obstruction being caused to users of the public highway and in the interest of public safety (Policy 9 of the Unitary Development Plan 2007: Policies saved beyond 5th August 2010 and not superseded by the Core Strategy: January 2011).

14. Details of all air conditioning units, ventilation and filtration equipment and any other plant, machinery or equipment (including rooftop plant), in addition to measures to control noise from such equipment, shall be submitted to and approved in writing by the Local Planning Authority prior to installation. Development shall take place fully in accordance with the approved details and shall be so retained for the duration of the permitted use unless otherwise agreed in writing by the local planning authority

Reason: To ensure that no nuisance or disturbance is caused to the detriment of the amenities of adjoining occupiers or of the area generally.

15. No plant shall be installed until an acoustic report has been submitted to and approved in writing by the Local Planning Authority. The report must set out the sound level generated from all noise generating plant and equipment and state the noise control measures to be employed to ensure the noise from the combined plant does not exceed a level of 10dB(A) below typical background noise levels, measured as L(Aeq-15mins), at the façade of the nearest residential/noise sensitive property.

Reason: To protect the local amenity from noise and disturbance.

16. Parking and turning facilities shall be provided fully in accordance with the details hereby approved and shall be constructed before the development is

occupied and shall be maintained for this purpose for the lifetime of the development.

Reason: To ensure that the development complies with Unitary Development Plan Policies and does not prejudice conditions of safety or traffic flow on adjoining highways.

17. Not less than 10% of all hotel rooms shall be provided to wheelchair accessible standards prior to the commencement of use, and thereafter shall be permanently maintained.

Reason: To provide an accessible development in accordance with Development Management Policy 31 and the London Plan.

18. Electric vehicular charging points shall be provided in accordance with the plans hereby approved (minimum of 6) and installed prior to the first occupation of the development and permanently retained thereafter.

Reason: To ensure that the development complies with sustainable development policy requirements of the London Plan.

19. The development shall not commence until details of a revised Sustainable Drainage Strategy based on an assessment of the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework. The Strategy shall include;-

- a. detailed investigation of existing ground conditions;
- b. detailing of any on and/or off-site drainage works;
- c. feasibility of above ground sustainable drainage solutions;
- d. further detail of green roofs;
- e. a permeable paving system; and
- f. feasibility of incorporating rain gardens/planters to provide treatment for the roof water runoff,

Details shall be submitted to and approved in writing by the Local Planning Authority. The drainage system shall be installed/operational prior to the first occupation and a continuing management and maintenance plan put in place to ensure its continued function over the lifetime of the development.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

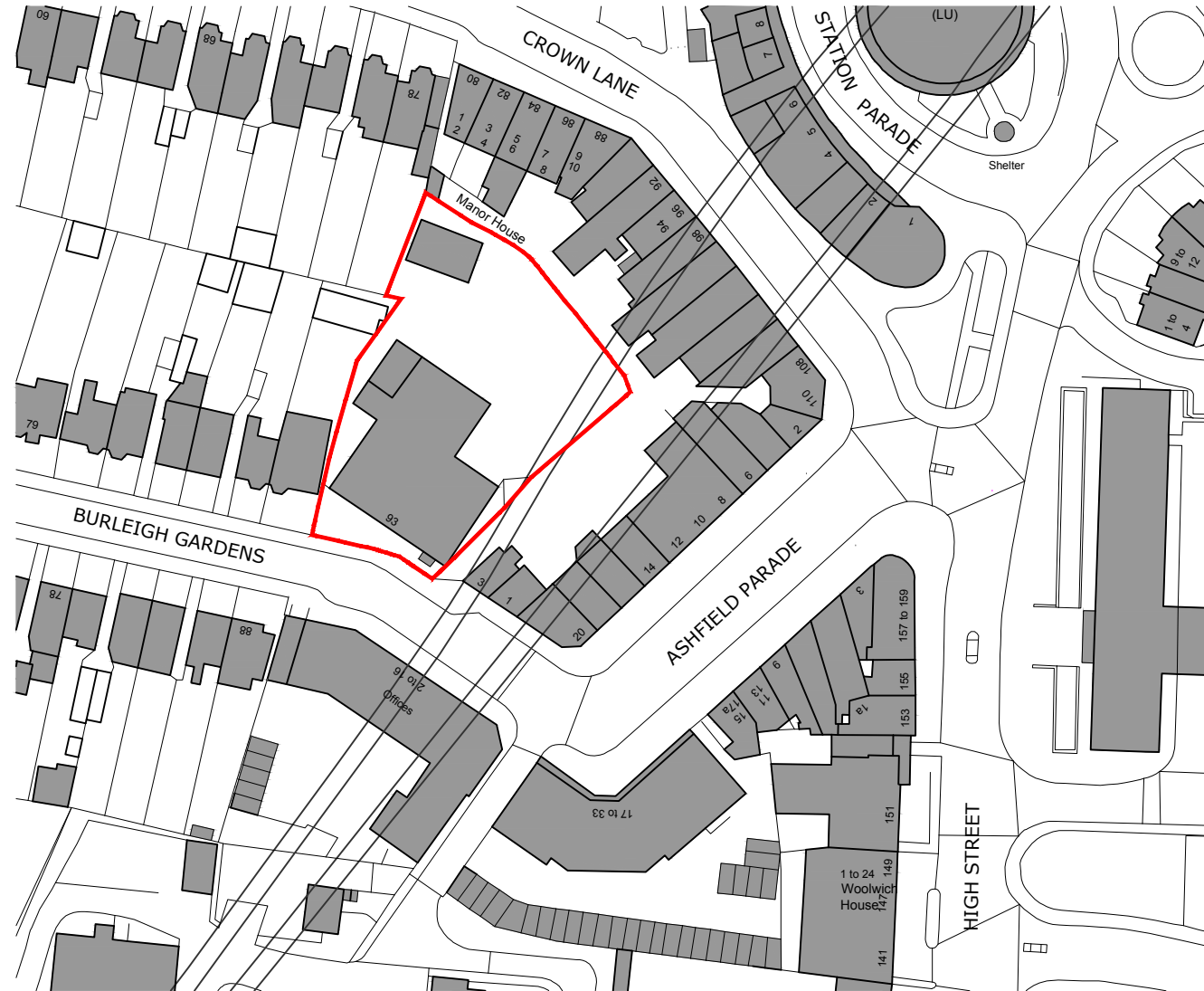
Reason: To ensure the sustainable management of water, minimise flood risk and to minimise discharge of surface water outside of the curtilage of the property in accordance with Policy CP28 of the Core Strategy, DMD61 of the Development Management Document, Policies 5.12 & 5.13 of the London Plan and the NPPF.

21. Details regarding the design, siting, height and degree of illumination of any external lighting within the site or external lighting to the buildings shall be submitted to and approved in writing by the LPA prior to installation.

Reason: To ensure submission of satisfactory details as well as ensuring the degree of illumination does not distract drivers or result in adverse light pollution.

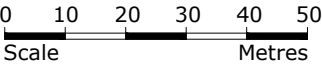
22. No external windows or doors other than those indicated on the approved drawings shall be installed in the development hereby approved without the approval in writing of the Local Planning Authority.

Reason: To safeguard the privacy of the occupiers of adjoining properties.



Do not scale from this drawing
The contractor is to check all dimensions on site and report any discrepancies to the architect
All rights described in chapter IV of the copyright, designs and patents act 1988 have been generally asserted

Notes



Legend:
Application Boundary



P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

3rd Floor
7-15 Rosebery Avenue
London EC1R 4SP
Telephone +44(0)2038746707
Website www.jeffersonsheard.com



Project
93 Burleigh Gardens

Drawing
Location Plan

Scale 1:1250@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-XX-DR-A-00001		Rev. P1

3.0

Design Proposals

3.1 Development & Design Brief

The design for the building and treatment of the site has been undertaken through careful analysis of the surrounding area and mindfulness of the planning committee comments associated with previous schemes.

The key aspirations of the brief are outlined below:

- Update the building facade to provide a high quality architectural development which reflects local style and values.
- Animate the front facade to create an attractive addition to the street scene based on local vernacular logic.
- Provide an improved interface between the front elevation/entrance and the immediate pavement.
- Refurbish the interior to a high standard to provide light and generous bedrooms, circulation and reception space.
- Introduce a new rear extension based on the scale supported in previous application P14-00048PLA.
- Reduce overlooking and increase privacy building-wide, especially towards residential neighbours along western boundary.
- Consider parking layout and landscaping at rear, introducing safe bike storage and refuse area in logical locations.
- Improve access/escape to and from building in line with altered internal layouts.



3.2 Concept Evolution

In the following sections the criteria by which JSA started to explore the external facade, especially the street scene frontage, will be explored.

The existing building is structurally sound with massing proportions that are consistent with the street scene along Burleigh Gardens. However, the identified lack of quality detailing or features of aesthetic interest and the monotonous use of windows provide a focus for improvement.

Our intentions in line with the brief involve the re cladding and treatment of the existing fabric to create a more attractive and positive contribution to the surrounding area that is based on principles of design evidenced locally.



Proposed concept development



Facade concept imagery



3.3 Massing Concept

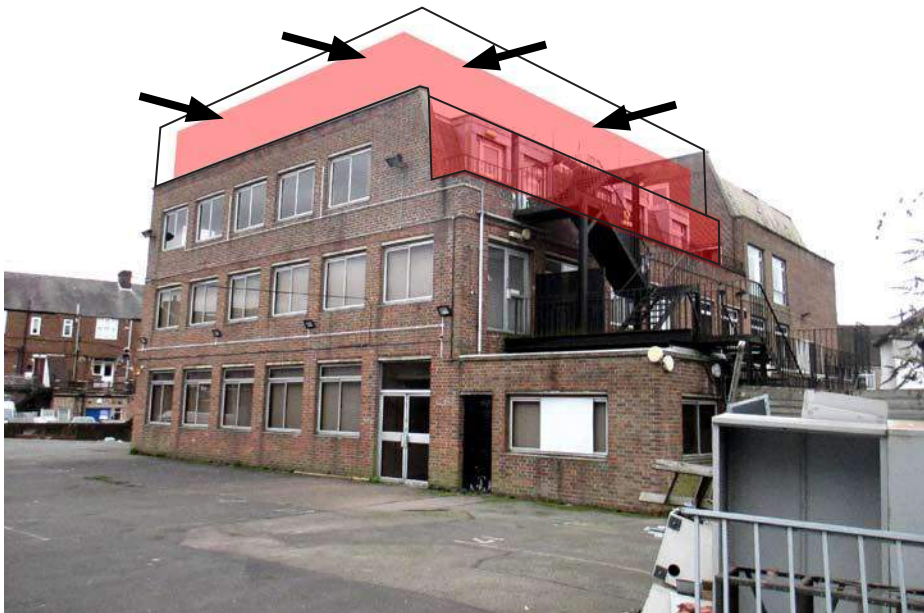
As previously discussed the introduction of an additional storey at the rear of the building was supported in planning committee feedback relating to previous application P14-00048PLA.

The intention is to match the scale previously suggested, erecting a flat roofed volume that reflects the height of the higher roof portion at the front of the building. Instead of maximising the footprint the new outline will be set back from the edge of the third floor parapet, increasing privacy and helping to reduce the perception of bulkiness.

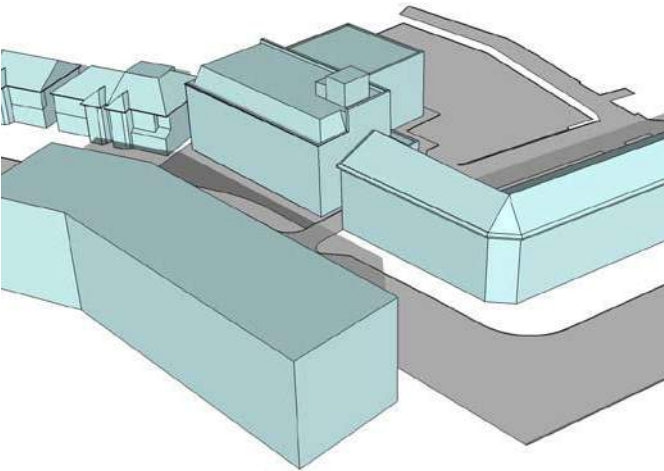
In addition, the existing third floor pitched roof portion on the north west side will be reconstructed on the same building line as existing to support the new level. This will provide a new parapet to fourth floor and reconfigured openings to suit the external escape stair and room layout.



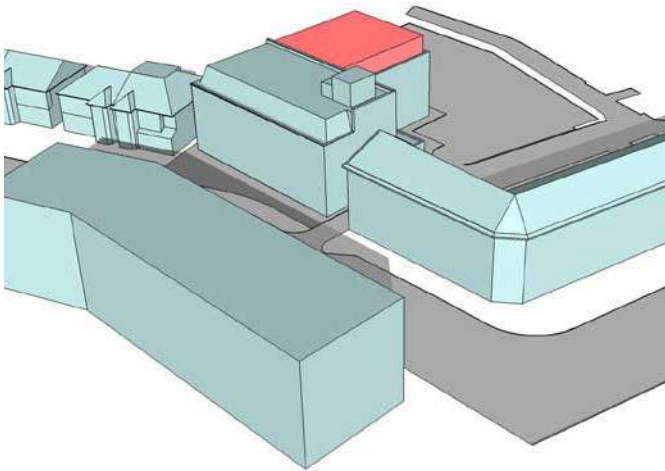
Additional Storey at Rear - Maximised footprint.



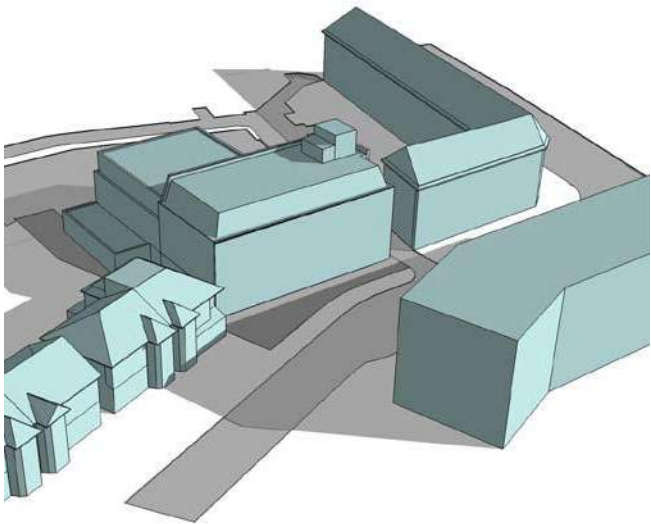
Additional Storey at Rear - Facade set back to increase privacy and reduce overlooking and impact.



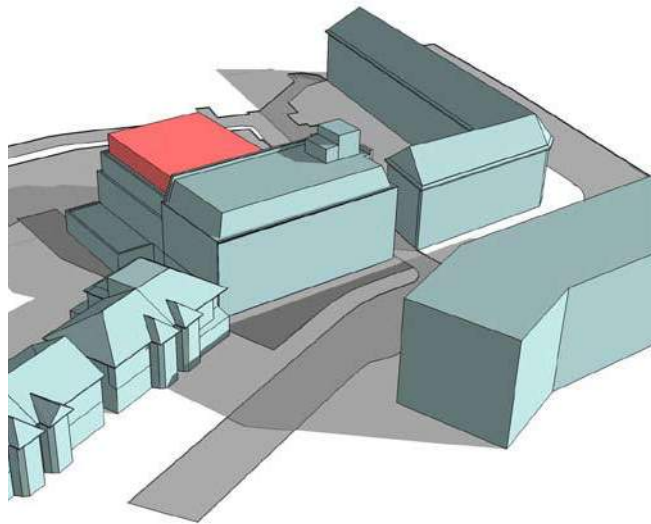
Bird's-eye (South) - Existing



Bird's-eye (South) - Proposed additional storey at rear



Bird's-eye (South West) - Existing



Bird's-eye (South West) - Proposed additional storey at rear

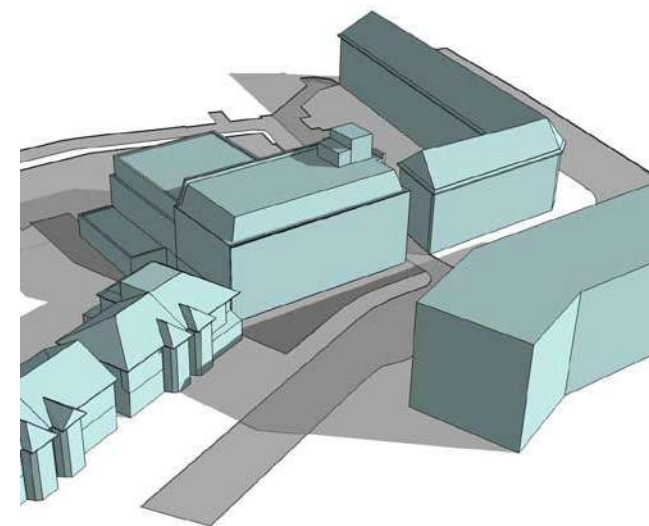
3.3 Massing Concept

As part of the internal layout improvements new lifts will be introduced and the existing plant room capacity will be bolstered to support a complete update of services.

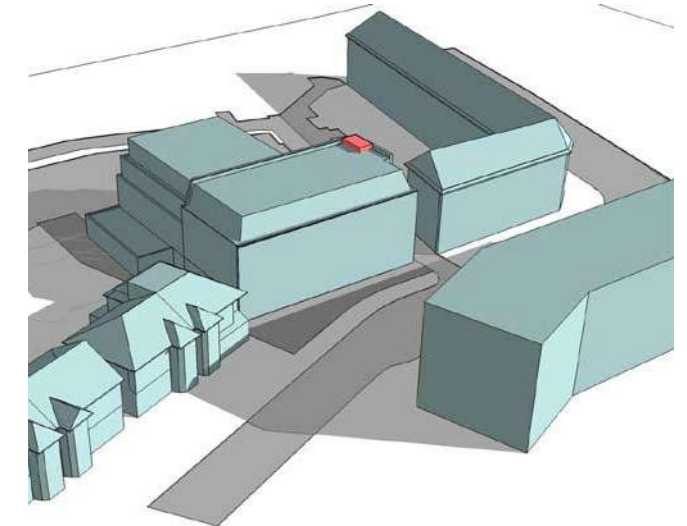
This will provide the opportunity to reconfigure the existing rooftop plant visible from street level. The proposed lift shafts will dramatically reduce the mass and lower the height of the flat roof structure, hiding it from view from below and realigning the building appearance with the surrounding neighbours.



Front Elevation - Existing lift shaft reduced in scale below level visible from street.



Bird's-eye (South West) - Existing



Bird's-eye (South West) - Proposed reduced lift shaft massing

3.4 Street Scene Concept

As a starting point for approaching the re cladding and reconfiguration of the external facade of 93 Burleigh Gardens our intention was to establish and analyse the language of other surrounding buildings, especially within the conservation area. From this we were able to identify principles for design which have been interpreted to better the contribution of the building toward the street scene and wider area as a considered, quality piece of architecture.



Local Vernacular - Vertical window and panel arrangement within Southgate Circus conservation area

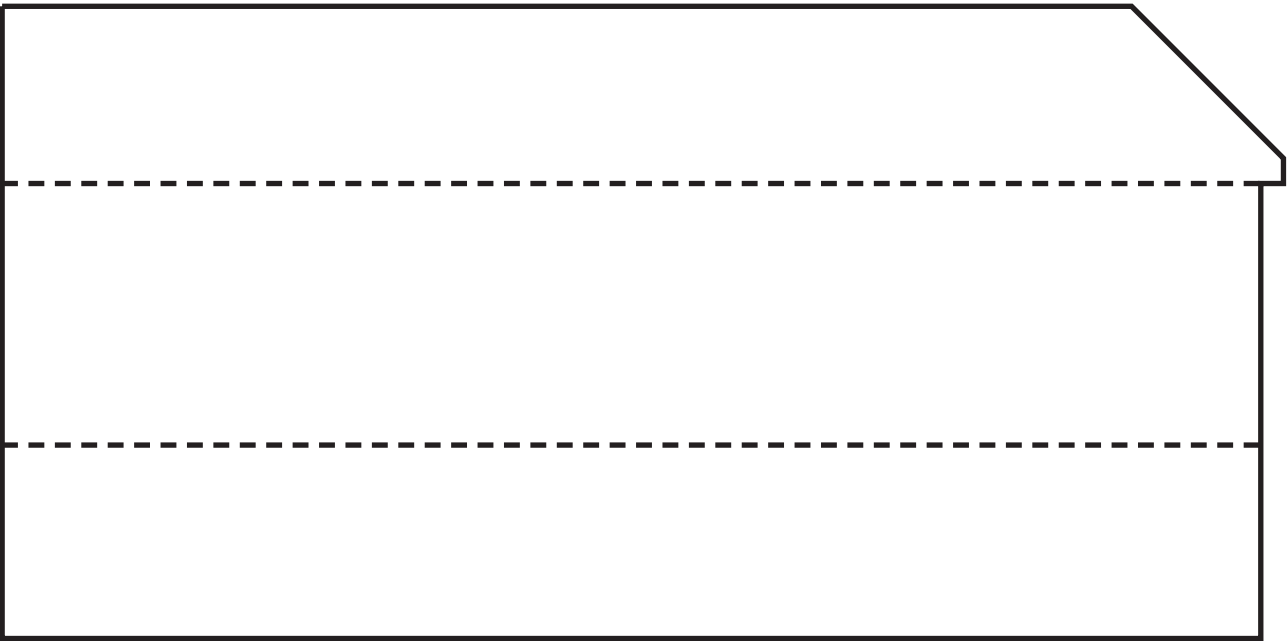


Local Vernacular - Horizontal ground floor plinth

3.4 Street Scene Concept

The neighbouring 1920's - 1930's properties along Ashfiled Parade to the east comprise of commercial A1 and A3 Class at ground floor with residential provision on upper floors. As described in the diagrams below these building frontages follow a definite pattern of configuration.

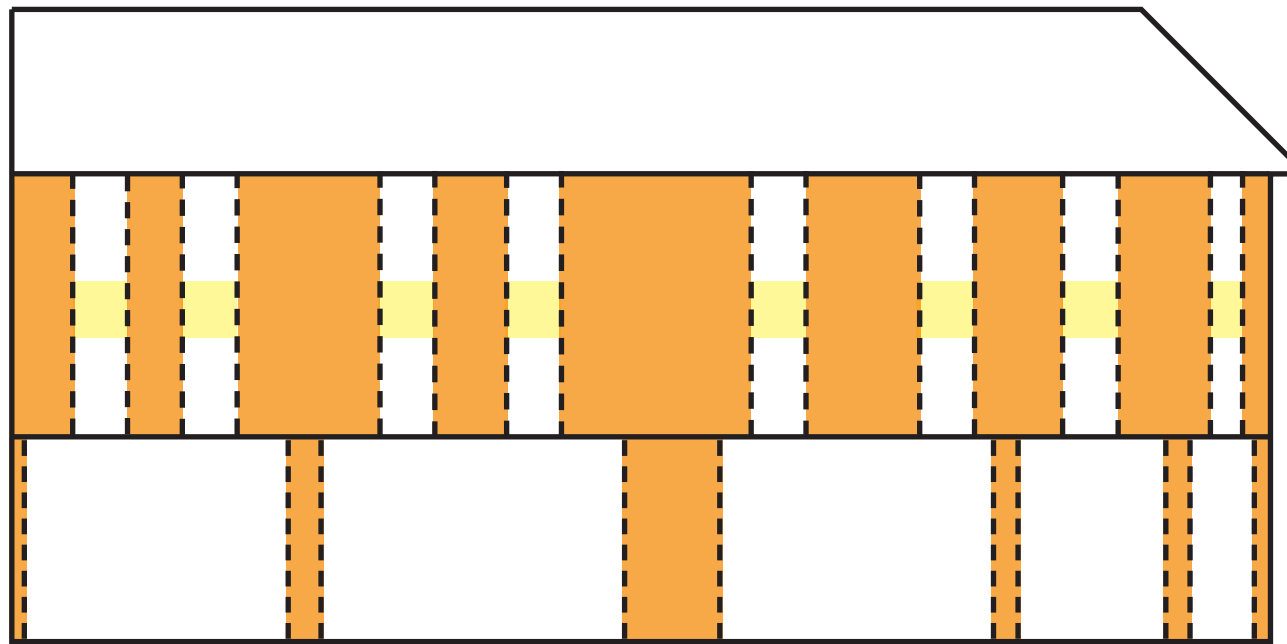
The ground floor at street level forms a plinth, contrasting in colour and material to the upper floors, and splitting the frontages horizontally by creating a datum. Windows on the upper floors are linked together via cladding panels and rendered surrounds to form elongated elements that divide the face in a series of vertical strips.



1 Facades are configured in a tripartite, horizontal arrangement with a defined plinth, middle and attic hierarchy.



It is proposed that the alterations to the proposed scheme's elevations will take inspiration from the articulation of neighbouring buildings frontages in respect to scale and proportions of architectural elements. The following diagrams illustrate the anatomy of these buildings facades.

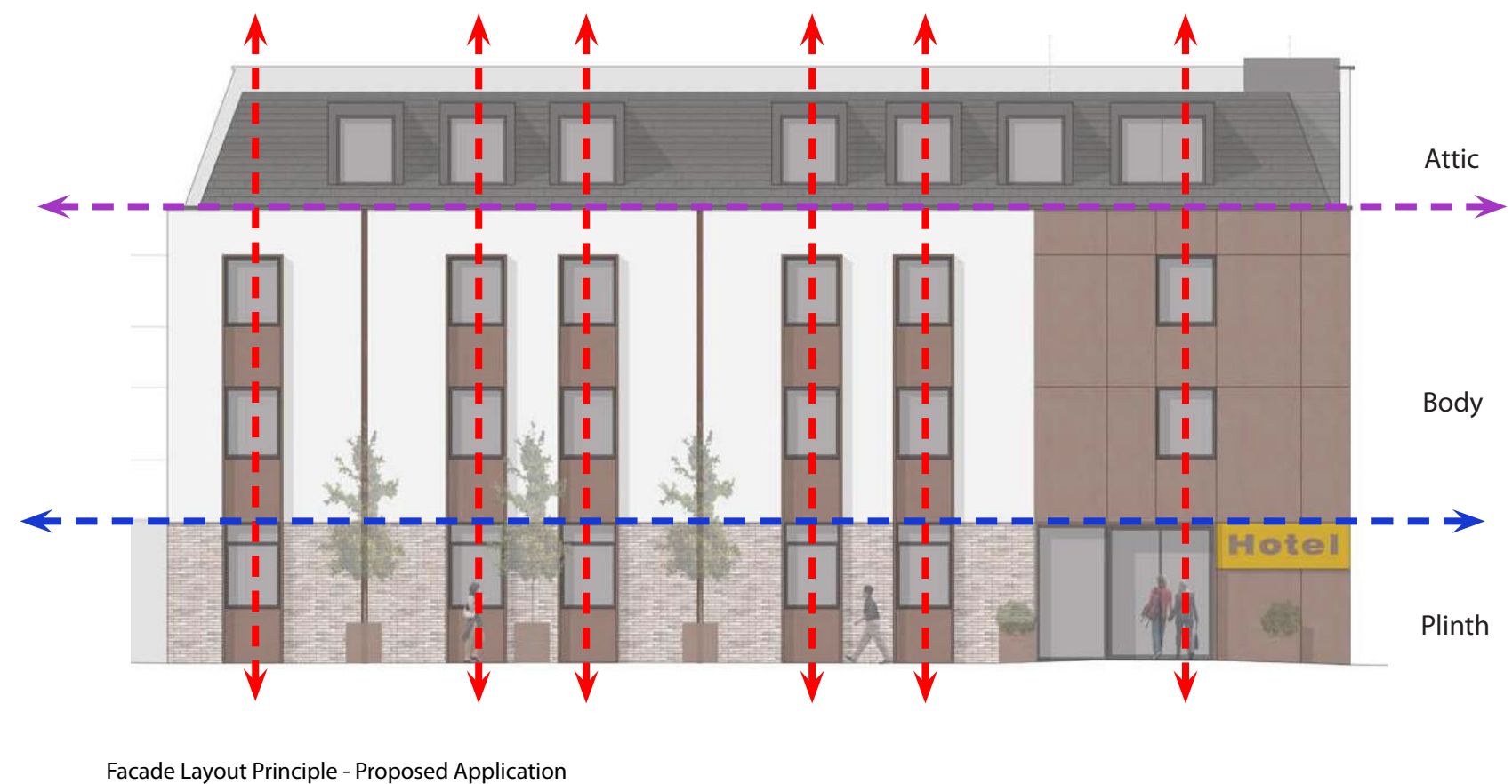
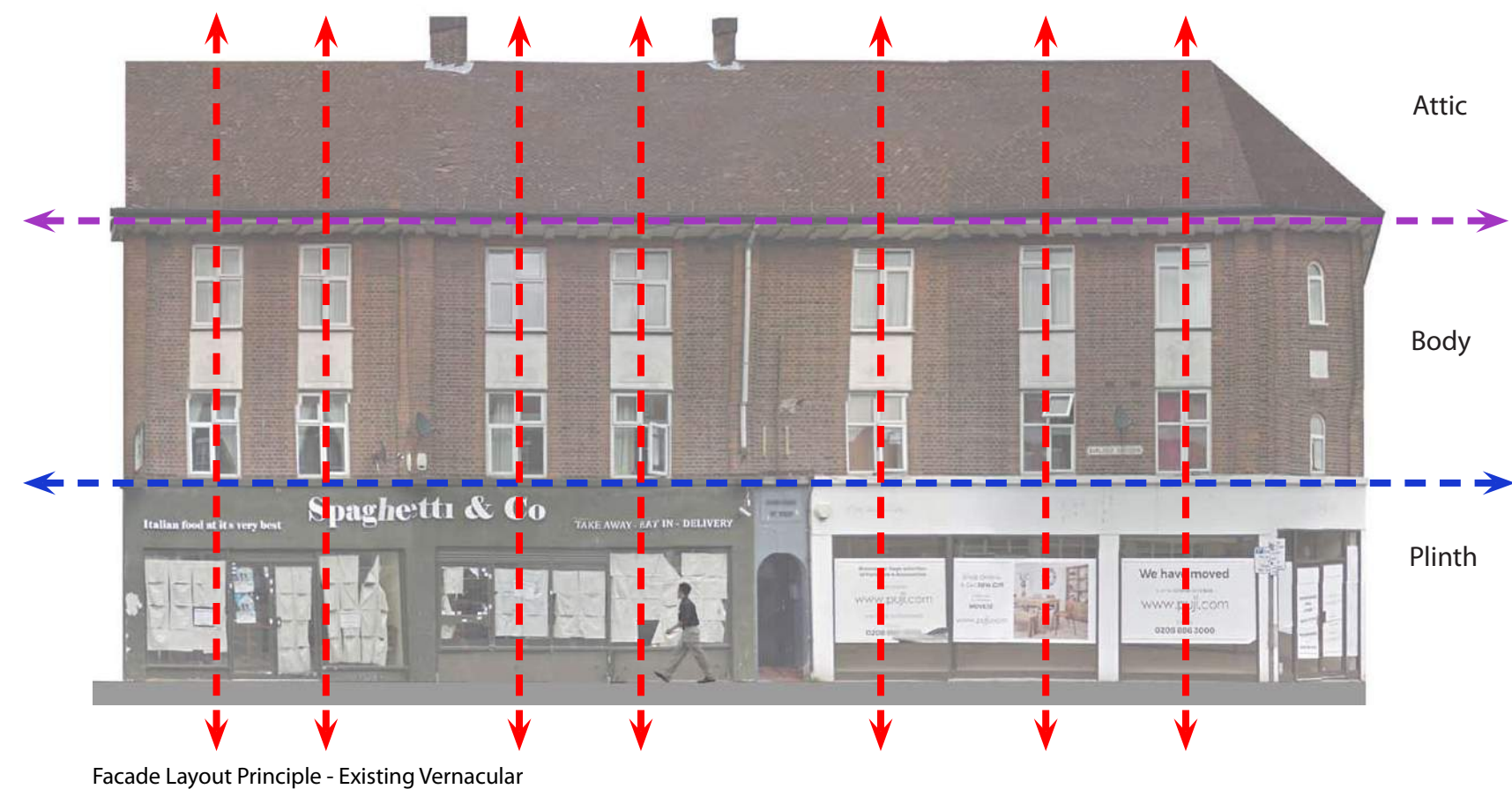


2 The illustration above defines the proportions of the vertical openings and the relationship of solid to void in that make up the building frontages.

Intermediate levels are vertically divided by grouped windows with contrasting spandrel panels providing a vertical rhythm across the elevations. The lower level is also fragmented by narrow piers that make up the shop fronts.

3.4 Street Scene Concept

By identifying a local language for facade set out, the following step was to apply this to our initial design development. The resulting proposed frontage creates a conceptual extension of the parade, implementing a different material at ground floor to create the horizontal plinth and windows grouped together vertically to replicate a familiar pattern.

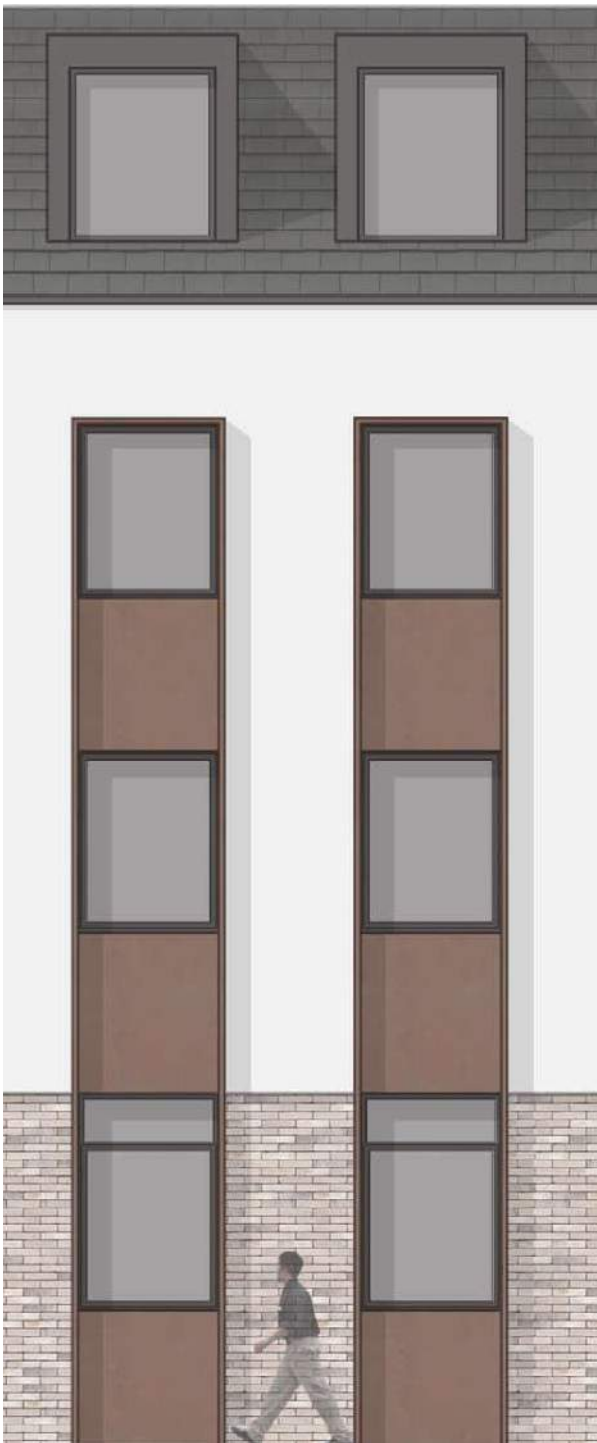


3.4 Street Scene Concept

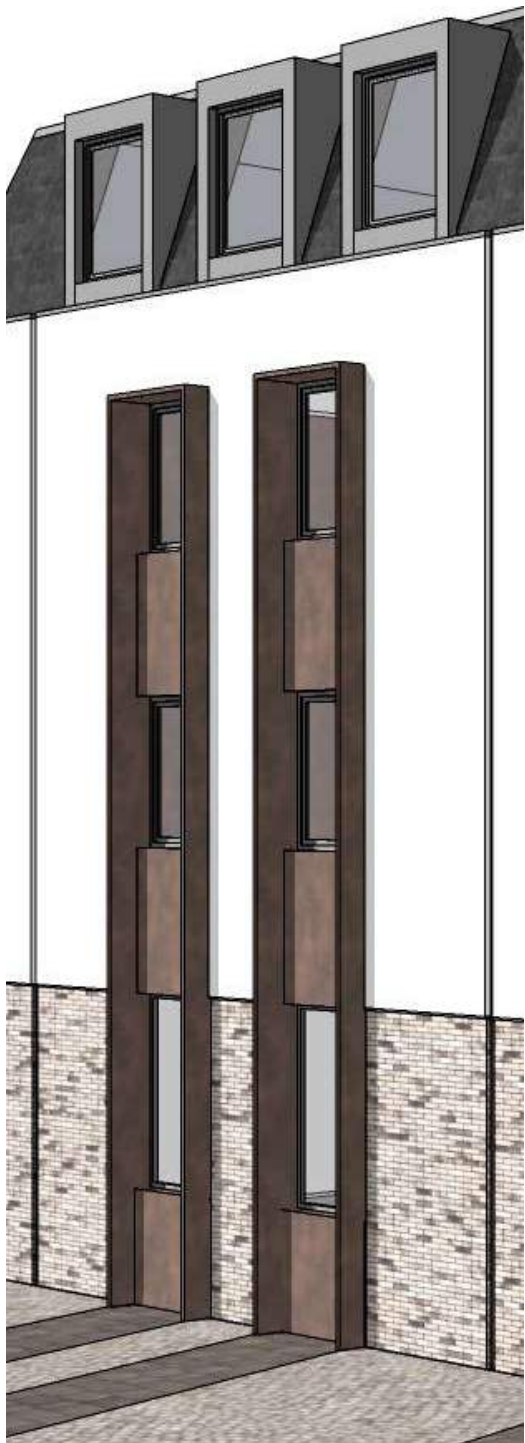
To encourage further interest and emphasise the concept of vertical window grouping, the proposal is to introduce protruded window reveals to the front facade. Extending from the head of windows at third floor down to ground level, the reveals will create subtle depth and shadow to what was otherwise a very flat and featureless face.



Window Reveal Precedents



Front Elevation Extract



3D Reveal Study

3.5 Overlooking Concept

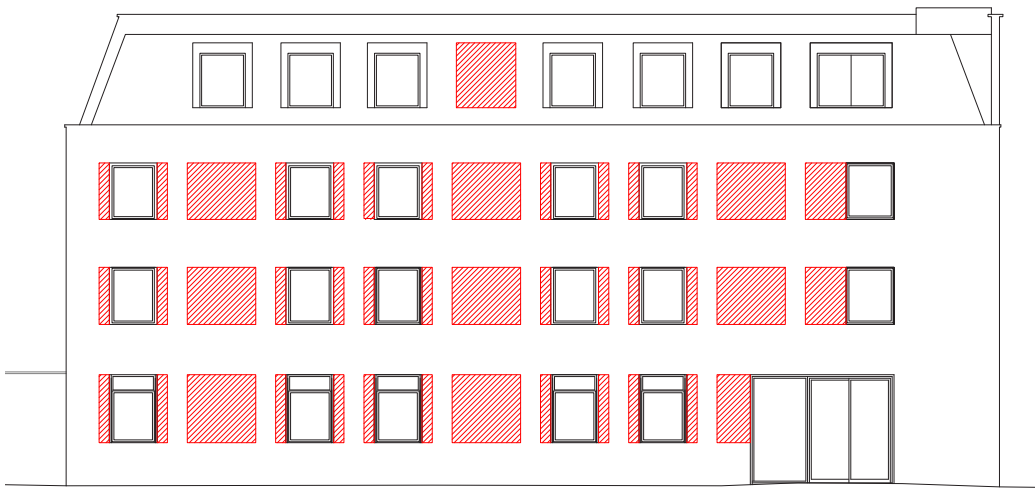
Due to the buildings change of use it has been an important part of the design approach to consider overlooking and appreciate both the neighbouring resident's and the hotel guest's right to privacy. The following pages demonstrate how window numbers, sizes and orientation have been adapted with these principles in mind.



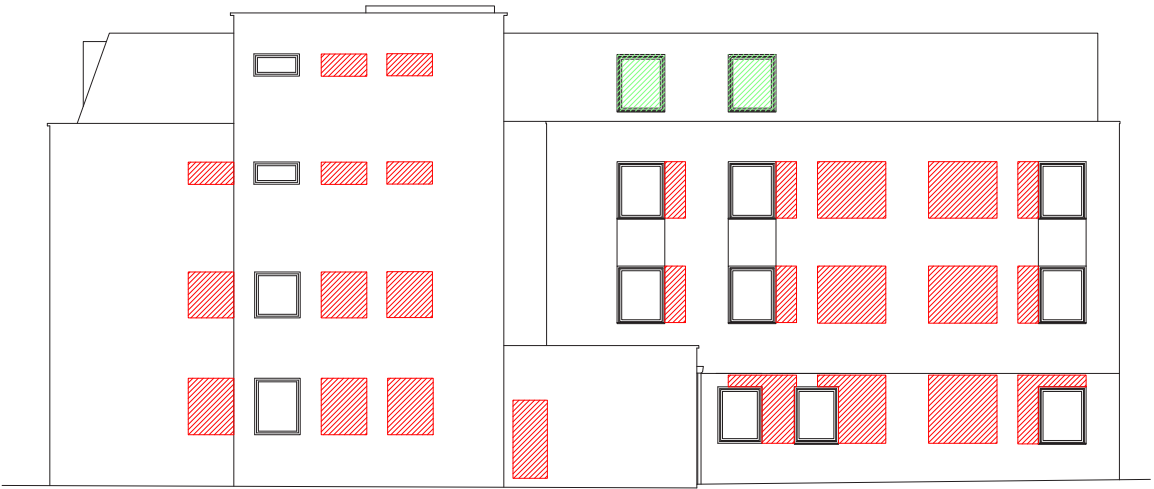
3.5 Overlooking Concept

Due to proposed internal layout updates a large number of existing windows become redundant or fall between habitable spaces and can be removed. This not only provides the potential to aid breaking down the bulk of the facade but also greatly decreases the implied amount of overlooking from 93 Burleigh Gardens to its surrounding neighbours.

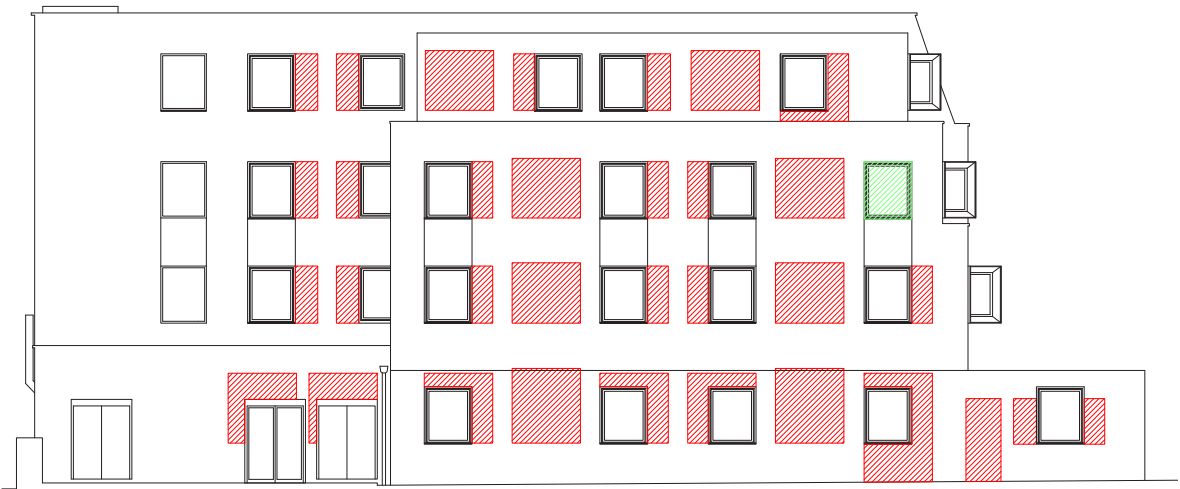
In line with the local facade principles identified, all existing window openings have also been reduced in width, emphasising divisive vertical lines but also the amount of glazing facing immediate residential properties.



Front Elevation (South West)

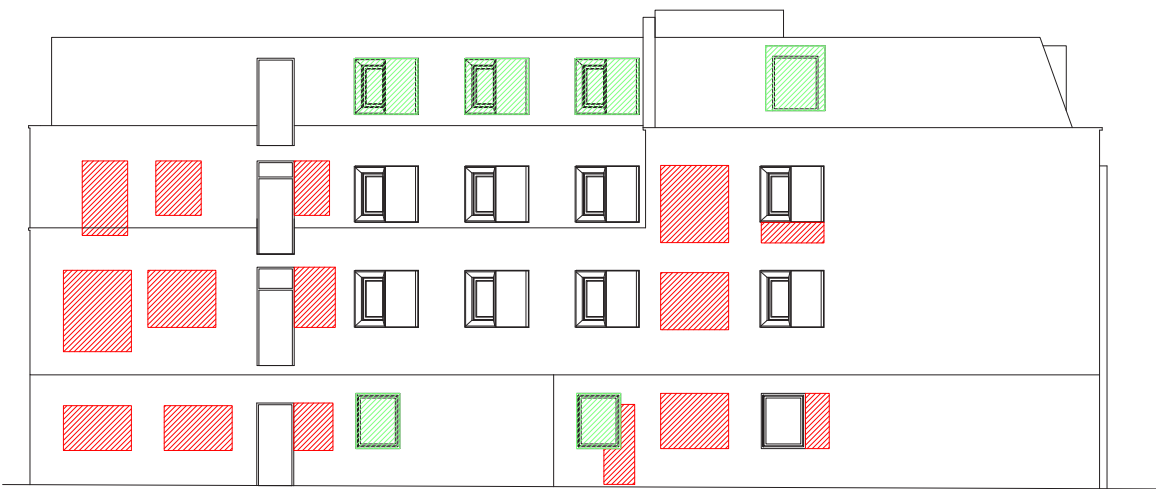


Side Elevation (South East)



Rear Elevation (North East)

Existing Opening Removed or Adapted █
New Opening Introduced █
*All other openings indicated are in existing positions



Side Elevation (North West)

3.5 Overlooking Concept

The most prominent existing issue with overlooking is from the side of the building toward the north west. A considerable number of existing large windows currently look directly out on the rear elevation and garden of no. 89/91, a semi detached two storey house.

The proposed scheme not only reduces the number and size of windows on this facade but also introduces angled window boxes which redirects the outlook from the hotel rooms to the north rather than north-west. A 45 degree orientation will prevent occupants having a direct view to the rear of houses along Burleigh Gardens and instead encourage sight lines towards the rear garden boundaries.



Existing view of no. 91 from first storey



Existing view of no. 91 from first storey

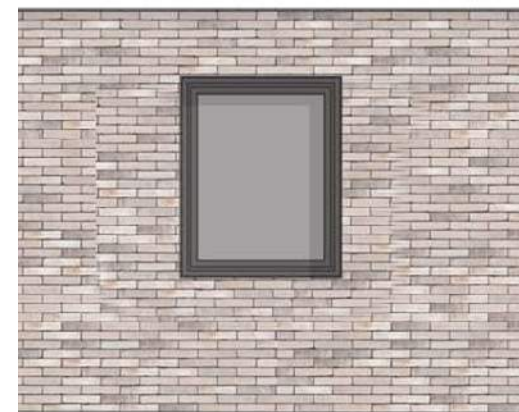
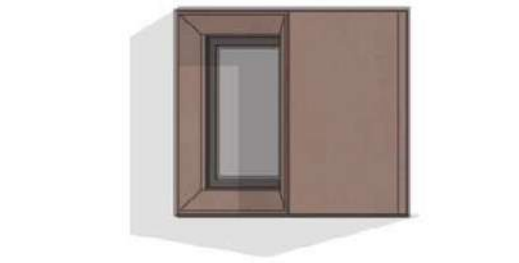
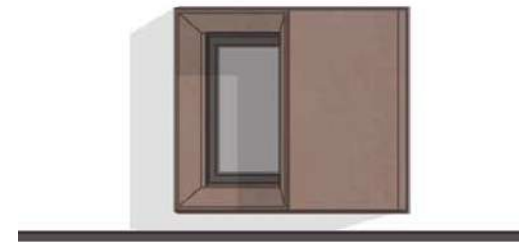
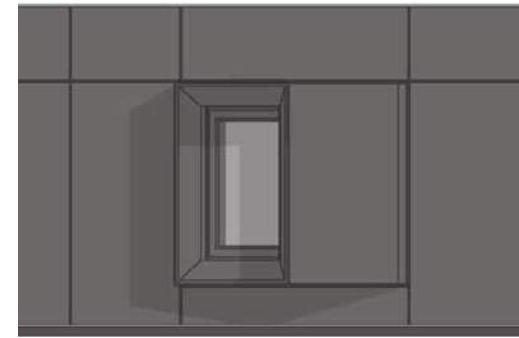
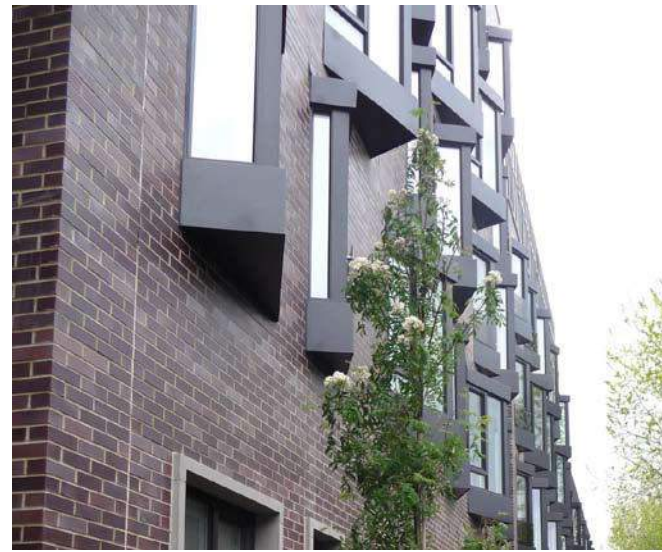


3.5 Overlooking Concept

The images to the right illustrate the nature of the angled windows on the north-west elevation to demonstrate the difference between the proposed and existing outlook of the north-west elevation.

Glazing from first, second and third floors is hidden when viewed from the rear elevations of the houses along Burleigh Gardens.

The images below show some precedents of angled windows implemented in different circumstances.



Proposed north west elevation extract



Proposed view from houses along Burleigh Gardens

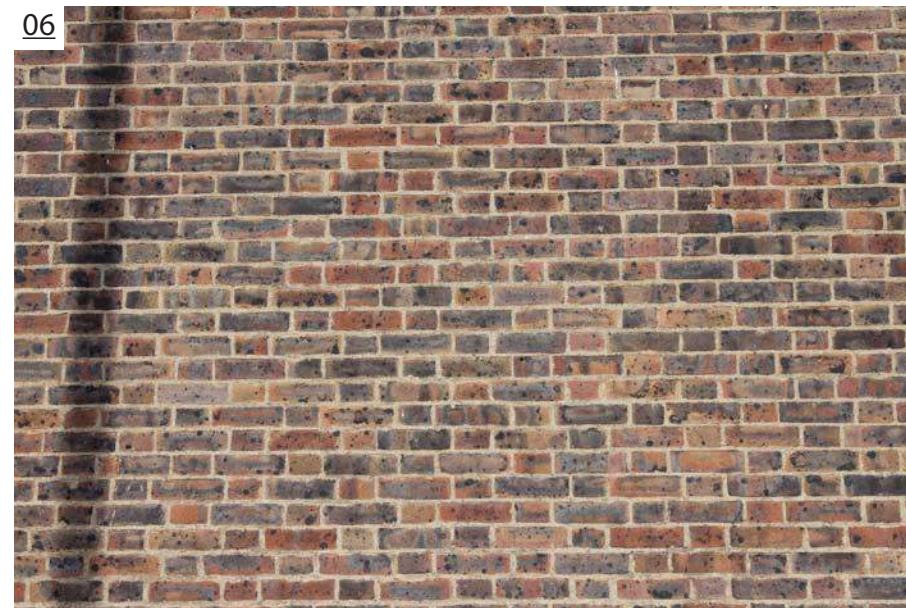
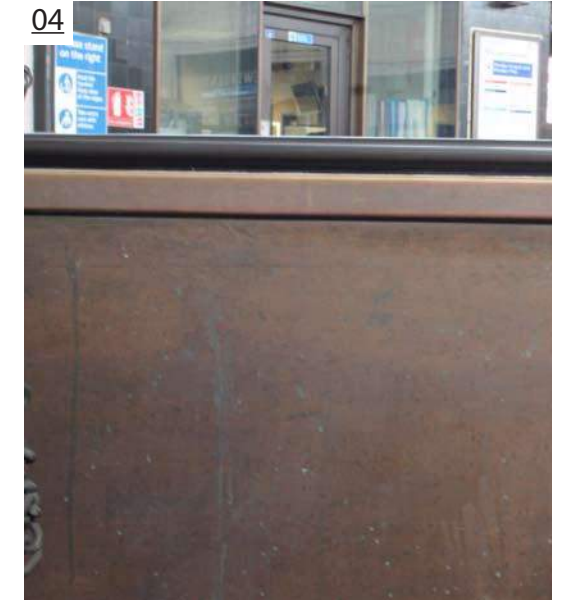
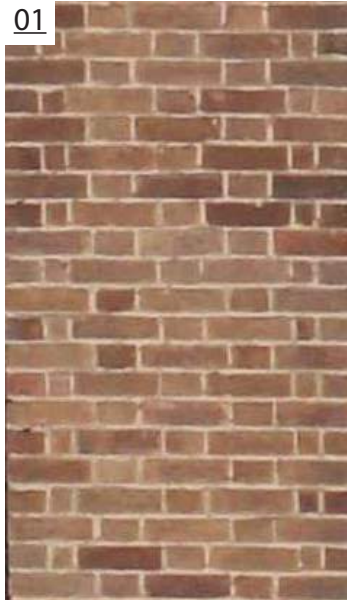


Angled window precedents



3.6 Material Concept

Local Material Precedents

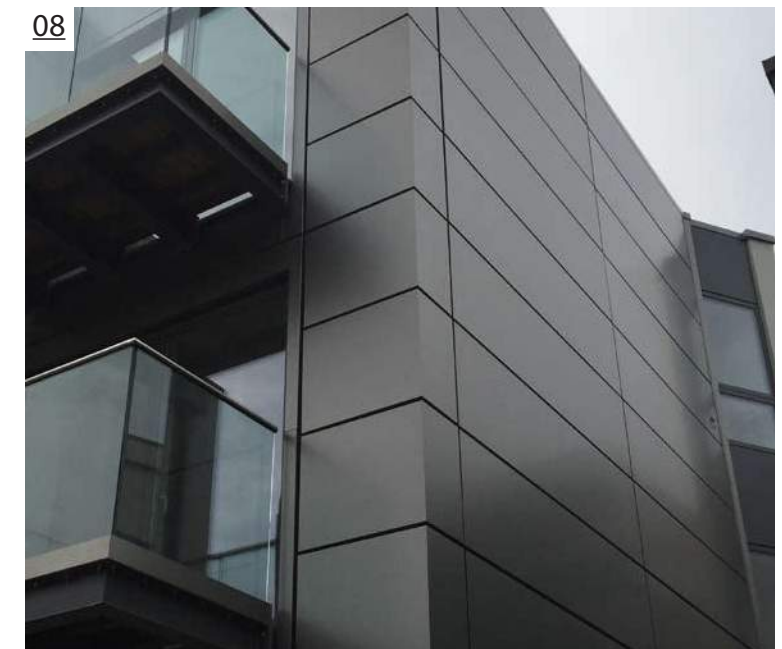
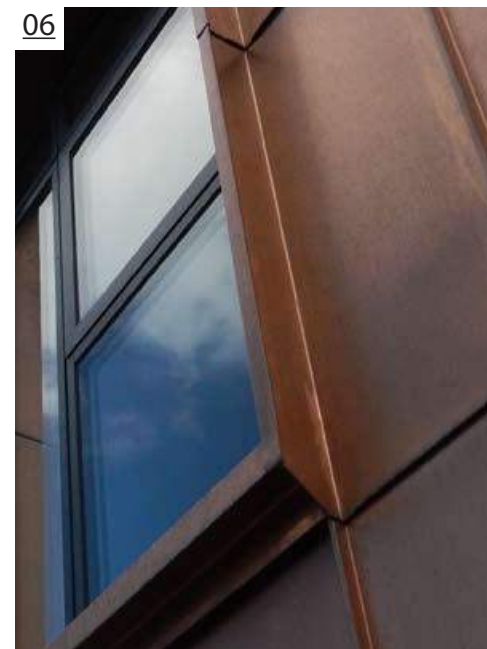


- 01 Yellow brickwork along Ashfield Parade
- 02 Bronze detailing within Southgate Station
- 03 Oxidised bronze/copper roof panels at Southgate Library
- 04 Bronze panelling within Southgate Station
- 05 Rendered facade at High Street Post Office
- 06 Red/Brown brickwork along Ashfield Parade
- 07 Bronze panelling along commercial frontage at Station Parade

Through first exploring existing local material use, a palette was assembled to represent, compliment and enhance the character of the area. This will allow the building to present a clear and informed logic.

3.6 Material Concept

Proposed Material Palette



01 Yellow London stock brick

02 Off white render

03 Lead sheet

04 Roof slates

05 Composite bronze panel

06 Composite bronze panel and window reveal

07 Composite Bronze signage panel

08 Grey rainscreen cladding

Brickwork - used to clad the ground floor plinth, yellow stock reflects numerous examples present locally.

Render - used mainly on upper floors, white render takes influence from residential properties along Burleigh Gardens.

Bronze - evidenced on the iconic station building, bronze panels provide warmth and interest around windows and on feature front facade.

Rainscreen - influenced by the slate tile colour of the existing fourth floor roof, grey cladding will provide a clean finish to the new extension.



Front Elevation (South West)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |



Side Elevation (South East)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |



Rear Elevation (North East)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |



Side Elevation (North West)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |

4.0 Visualisations

4.1 Visualisation



4.2 Visualisation



5.0
Access & Sustainability

5.1 Access Statement

Below is a summary of the main access and operation considerations. For more detailed information please refer to the Transport Statement prepared as part of the overall submission by Transport Dynamics.

Pedestrian Access:

There are two main entrances to the building. From both front and rear the arrival points lead directly into the reception lobby with clear circulation routes and access to the stair and lifts.

Means of Escape:

Two stairwells, accessible from all levels, at opposing corners of the building provide safe escape in event of fire. One external and one internal, both lead to the ground floor level and out on to the car park and street side respectively.

Refuge:

The internal escape stair provides refuge space for a wheelchair at all floors in line with Building Regulations Part B guidance.

Guest Vehicle Access:

Access to the car park is via the existing shared route alongside the building. A newly configured layout provides spaces and circulation in line with Building Regs Part M.

Service Access:

Access through double doors at the rear leads directly to back of house areas including the linen store. Deliveries and collections can be made without impeding hotel guests.

Refuse Collection:

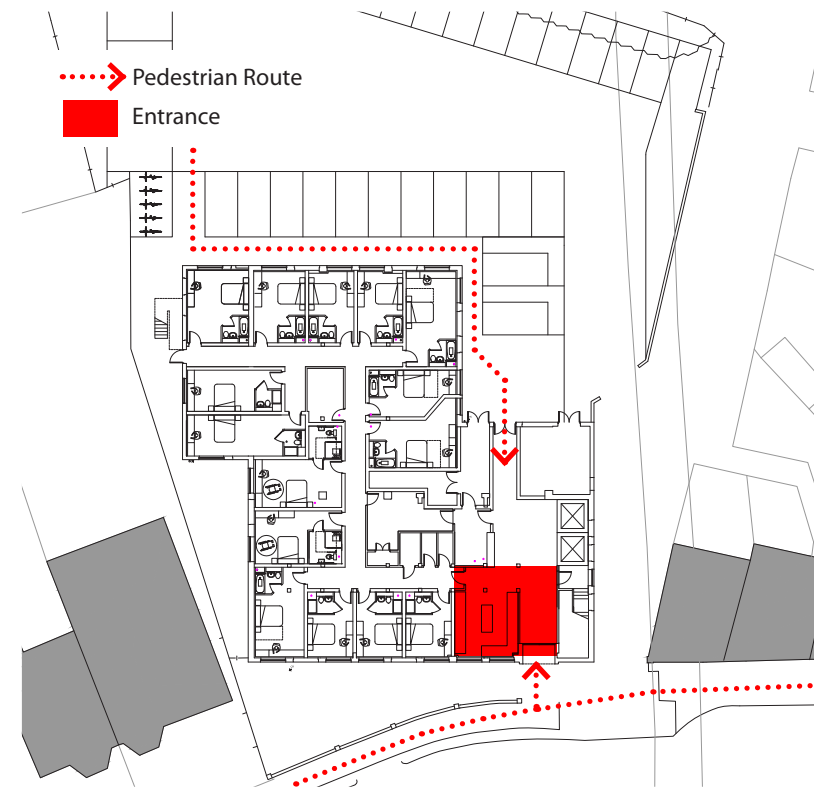
The existing building strategy is maintained, with collection vehicles accessing via the shared driveway. A new bin store located on the eastern boundary of the car park will provide close proximity for collection and hotel use whilst being far enough away from habitable rooms.

Cycle Parking:

A covered shelter with Sheffield bike stands will be provided and accessed via the car park. No.s will be based on TFL design standards.

Inclusive Design:

To aid access for all a number of considerations have been implemented including lift access to all floors, level thresholds throughout at ground floor, automatic entrance doors, corridor widths of 1200mm minimum, 1500mm dia turning circles, disabled parking spaces and dedicated wheelchair accessible bedrooms.



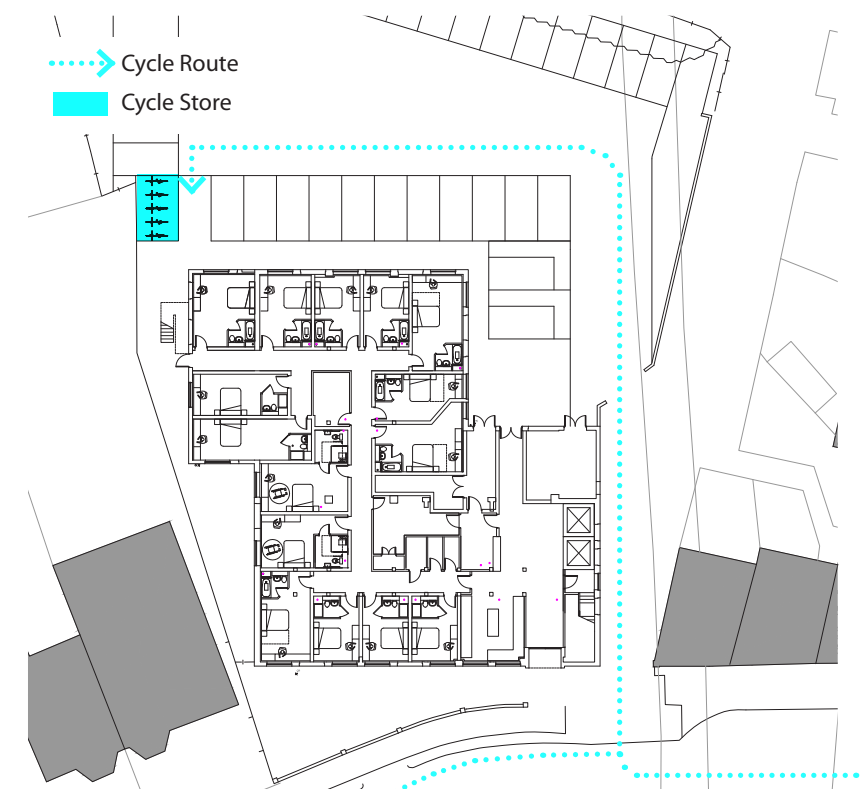
Hotel resident Access



Refuse Collection / Service Access



Means of Escape



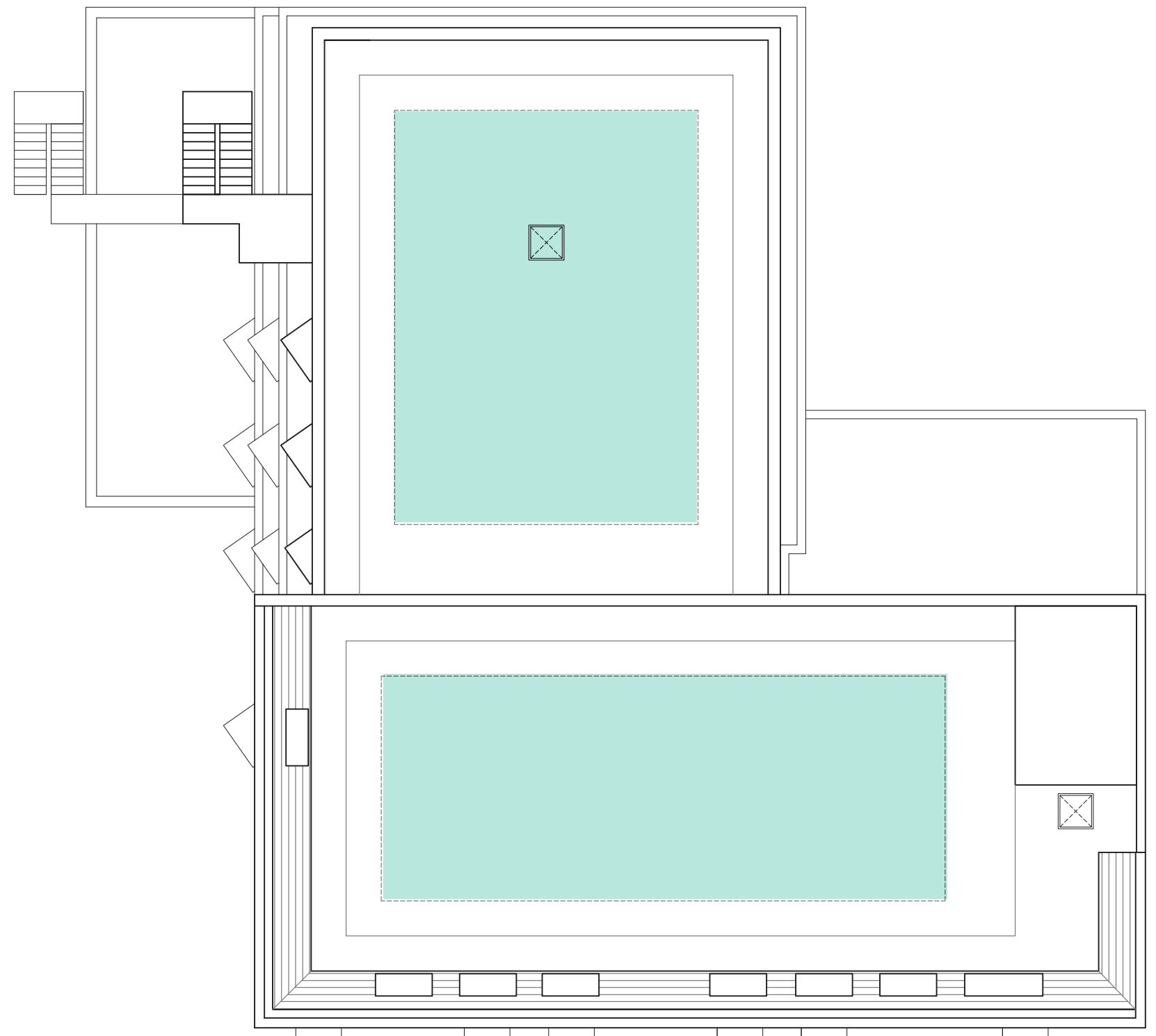
Cycle Store Access

5.2 Sustainability

The modifications to the existing building will aim to minimize its carbon footprint and achieve the highest feasible and viable sustainability standards taking account of the constraints posed by the existing structure. This will be sought through implementation of:

- Efficient low energy lighting incorporating use of LED fittings.
- Replacement of existing windows with highly efficient double glazed units which improves thermal and acoustic performance.
- Upgrades to the thermal fabric of the existing building with improved U-Values that comply with the minimum standards set out in Building Regulations Part L1B.
- Use of photovoltaics to meet target renewables requirement thus providing a reduction in carbon emissions.
- Use of low energy appliances where possible.
- Improved levels of air leakage from the existing building.

In addition to the above ample cycle storage is to be provided which encourages a more sustainable mode of transport. Suitable means of waste and recycling storage is also considered within the proposals.



Indicative extent of proposed solar thermal / PV panels

6.0

Area & Amount

6.1 Area & Amount

	Ground Floor		First Floor		Second Floor		Third Floor		Total	
Area	m²	ft²	m²	ft²	m²	ft²	m²	ft²	m²	ft²
Existing GIA	656.7	7,068	538.2	5,793	523	5,629	266.5	2,868	1,984.40	21,358
Proposed GIA	656.7	7,068	538.2	5,793	530.7	5,712	470	5,059	2,195.60	23,633
Difference +	0	0	0	0	7.7	83	203.5	2,191	211.00	2,275.00
Room Type	No.s		No.s		No.s		No.s		Total	
Double	10		12		14		12		48	
Family	3		4		2		2		11	
Accessible	2		2		2		1		7	
	15		18		18		15		66	
Parking Provision										
Cars Spaces									31	
Disabled Bay Spaces									2	
Cycle Spaces									6	

Project

0883 - 93 Burleigh Gardens

Subject

Design and Access Statement

May 2017



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1.0 Introduction

1.1 Purpose of Scope

This Design and Access Statement has been prepared by Jefferson Sheard Architects (JSA) on behalf of Palmers Green Investments Ltd (PGIL) to support a detailed planning application for a change of use hotel scheme.

This design statement aims to demonstrate an analysis of the site at no.93 Burleigh Gardens, considering context through to the design approach for the remodelling and refurbishment of a former B1/D1 class building into a new hotel use.

This document is to be read in conjunction with accompanying drawings and reports in support of the planning application.



1.2 Outline Brief

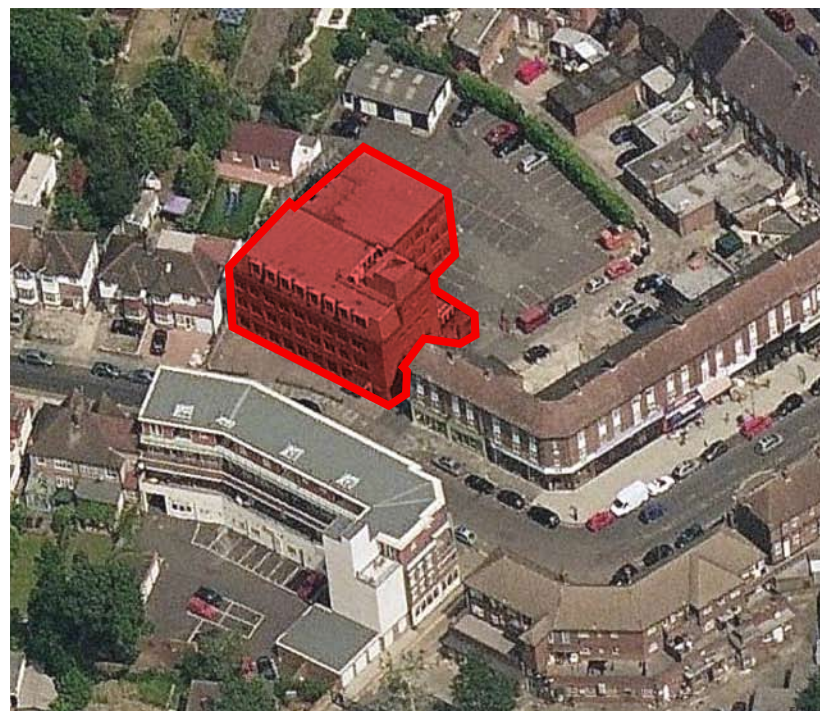
The client's ambition is to improve the buildings contribution towards the character of the street whilst providing a valuable new local amenity to Southgate.

The change of use from B1/D1 Class Office and Education facility will comprise of complete internal and external refurbishment and reconfiguration including a new roof level extension to the rear of the property. The building fabric will be updated and new materials and facade configuration implemented to provide an attractive addition to the street scene that echo the existing local vernacular.

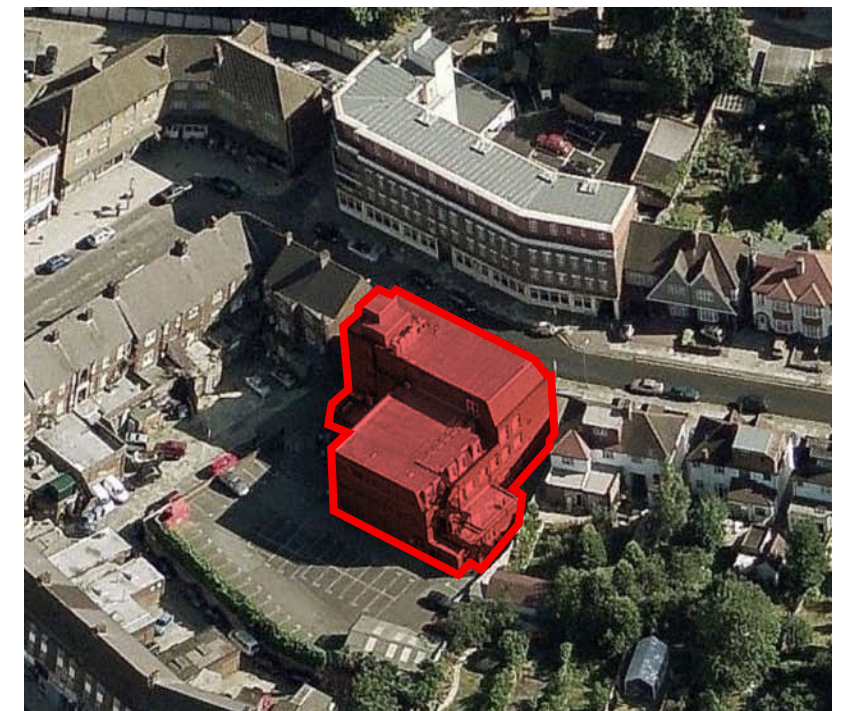
1.3 Site Location

The site is located on Burleigh Gardens and has strong public transport links. Southgate Underground Station (Piccadilly Line) and multiple bus stops along Station Parade are both within a 2 minute walk to the north east. National rail stations are also located close by at Winchmore Hill and New Southgate.

Access to the A1 motorway is 2 miles to the east and the M25 12 miles to the north.



Bird's-eye View From South



Bird's-eye View From North

1.4 Planning Policy

For full planning policy consideration please refer to the separate Planning Statement prepared by Five Development Consultancy LLP submitted as part of the full application.

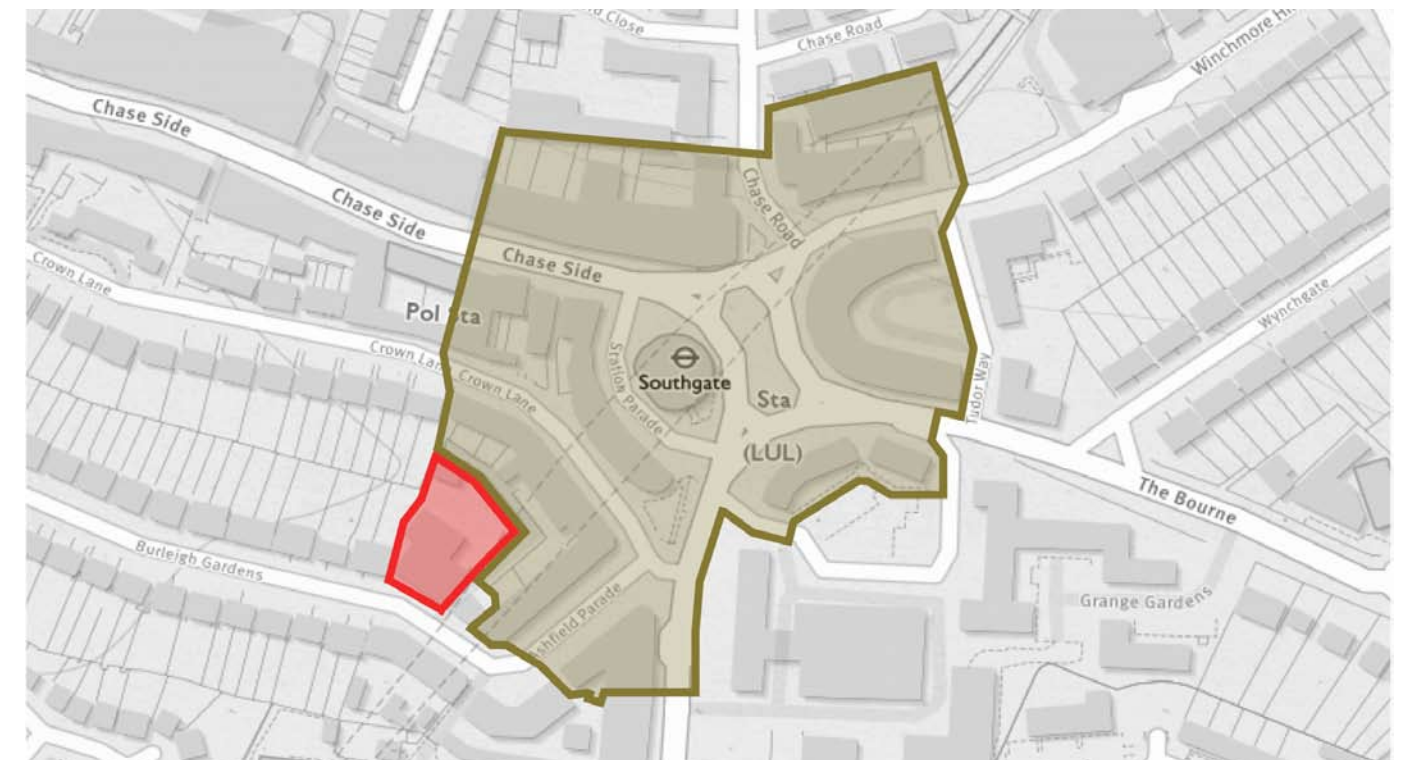
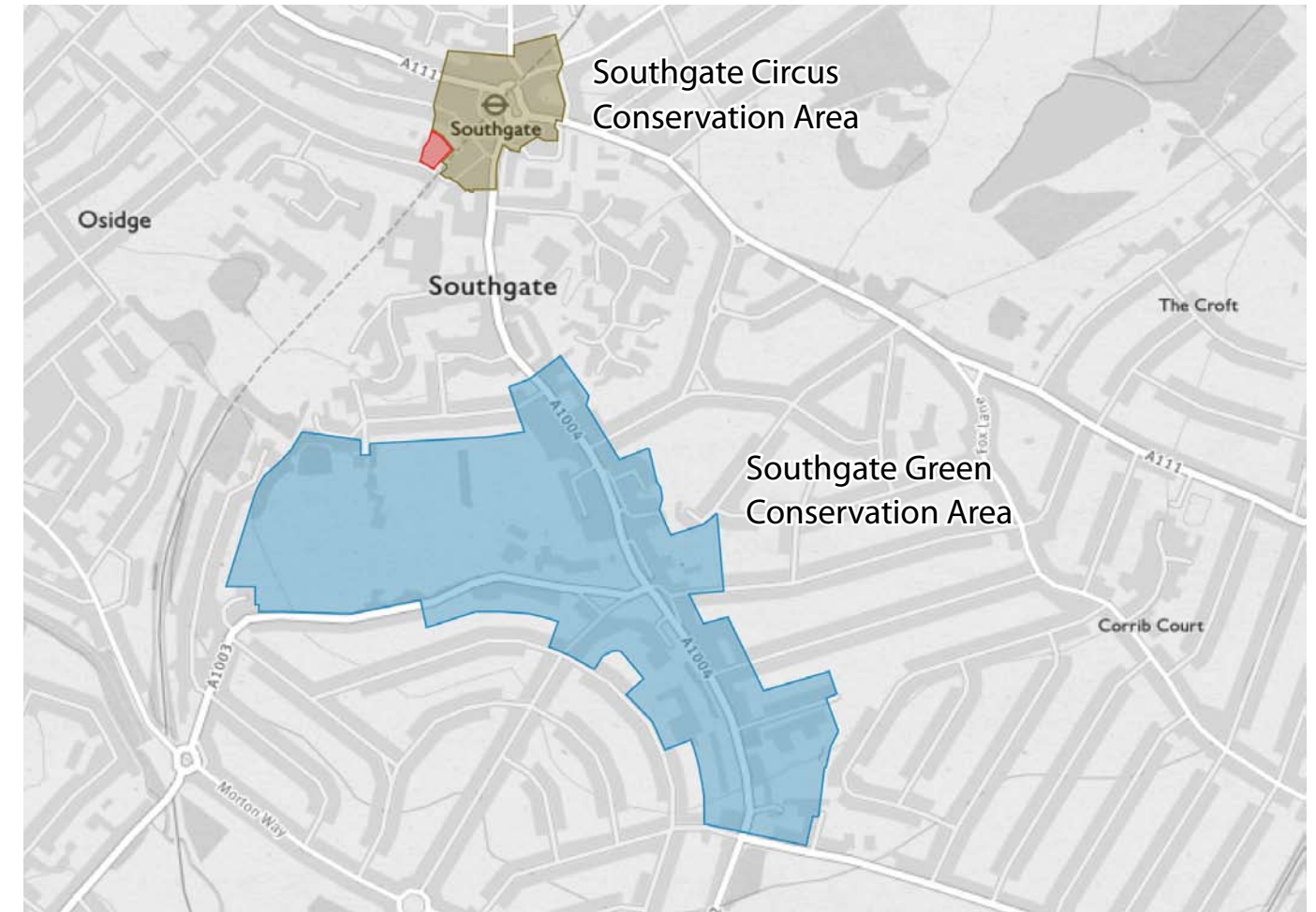
The list below demonstrates in summary the documents utilised to justify and determine the criteria for hotel development:

‘The National Planning Policy Framework’

‘The London Plan, Spatial Development Strategy for Greater London, 2015’

‘The Enfield Plan, Core Strategy (2010)’

‘Enfield Development Management Document DMD (2014)’



Southgate Circus Conservation Area



1.5 Planning History

A previous application (P14-00048PLA) relating to the conversion of the building for residential use (Use Class C3) received constructive feedback via planning committee. Although in this case the decision was refusal there were a number of key observations which have been used to form the basis of this new scheme.

Below are a number of extracts from the report (dated 25.03.14):

Extension

'The proposed extension works are sited to the rear of the site and will be largely screened from the street scene therefore are not considered to disrupt the character of the surrounding area.'

Massing

'The proposed fourth storey extension at the rear in terms of scale and mass is not considered to cause significant impacts on the neighbouring properties.'

Front Elevation

'This will include the introduction of large panels of render. It is considered that this form of external finish, creating a striped effect, is unnecessary and would appear a little incongruous within the street scene.'

Street Scene

'It is proper to seek to promote or reinforce local distinctiveness..... Consequently the external alterations are not considered to respect the character of the surrounding area and incongruous within the street scene.'

Overlooking

'Side windows will not be permitted unless they do not result in an adverse degree of overlooking and loss of privacy.....the proposed development with an increase in side facing fenestration will result in a significant increase of overlooking of neighbouring properties.'



Previous Application (P14-00048PLA) - Proposed Front Elevation (South West)

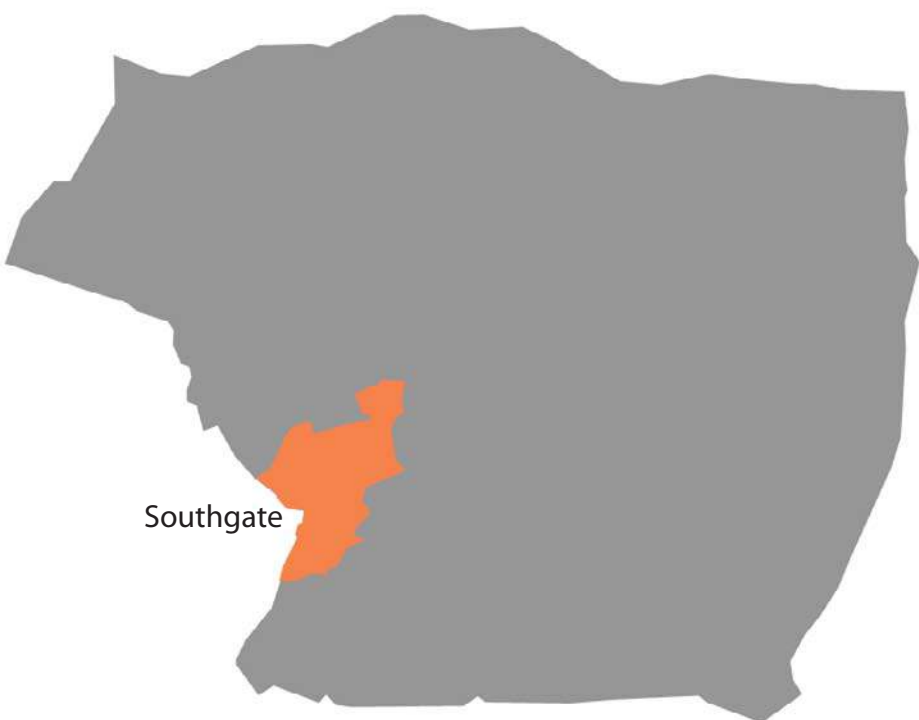


Previous Application (P14-00048PLA) - Proposed Side Elevation (North West)

2.0

Site Context & Analysis

2.1 Site Location



- | | | | | | |
|-----|--------------------|-----|-------------|-----|-----------|
| 1. | Hillingdon | 13. | Hackney | 25. | Southwalk |
| 2. | Harrow | 14. | Haringey | 26. | Lewisham |
| 3. | Barnet | 15. | Camden | 27. | Greenwich |
| 4. | Enfield | 16. | Westminster | 28. | Bexley |
| 5. | Waltham Forest | 17. | Kensington | 29. | Bromley |
| 6. | Redbridge | 18. | Hammersmith | 30. | Croydon |
| 7. | Barking & Dagenham | 19. | Brent | 31. | Merton |
| 8. | Havering | 20. | Ealing | 32. | Sutton |
| 9. | Newham | 21. | Hounslow | 33. | Kingston |
| 10. | Tower hamlets | 22. | Richmond | | |
| 11. | City | 23. | Wandsworth | | |
| 12. | Islington | 24. | Lambeth | | |

The site is located in the Borough of Enfield, North London, whilst the western site boundary line denotes the border with Barnet in which the residential properties further along Burleigh gardens sit.

2.2 Site Description

The existing building comprises of 4 storeys fronting Burleigh Gardens, stepping down to 3 storeys at the rear. Built predominantly in brick, the facades are punctured by a banding of windows arranged horizontally.

The upper storey takes the form of a mansard roof finished in a slate tile with projecting dormer windows. Roof top plant extends the height of the building located at the gable end of the eastern elevation.

The GIA of the existing building totals 1,984sqm with floor to floor heights of 2.4m>. The building's main entrance is at the front of the building with a secondary rear entrance at the rear north-eastern corner. An external, steel fire escape stair is located on the western side of the building which terminates in the car park.



Front elevation from Burleigh Gardens



Rear elevation from car park

2.2 Site Description

The exiting building does not make a meaningful contribution to the surrounding built environment and thus has little sensitivities with the neighbouring buildings within the Southgate Circus Conservation Area. The building's frontages reflect a more horizontal emphasis due to the banding of the windows which is not consistent with vertical arrangement and proportioning of the local vernacular. The existing façades also do not define the ground floor 'plinth' level which is strongly established in the Conservation Area.

The dated, utilitarian appearance of the building as well as crudely exposed incoming services that have over the years covered the building's facades, warrants proposals to improve the building's appearance and provides opportunities to better integrate it in with the local vernacular.

Many of the building's services are life expired and has a low energy performance with respect to thermal conductivity of the external walls, floor and roof. Proposals to refurbish and remodel the building inside and out will bring it back into commercial use.



View across existing car parking to outbuilding at rear



Existing external escape stair on side elevation (North West)



Eastern boundary refuse storage for commercial properties along Ashfield Parade



Existing access into car park via barrier arm and gate

2.3 Application Boundary

At the front of the building is a triangular piece of land defined by a low brick wall which sits within the application boundary. This is currently being used as informal parking. The car park at the rear is reached via a shared access road and serves the whole building.

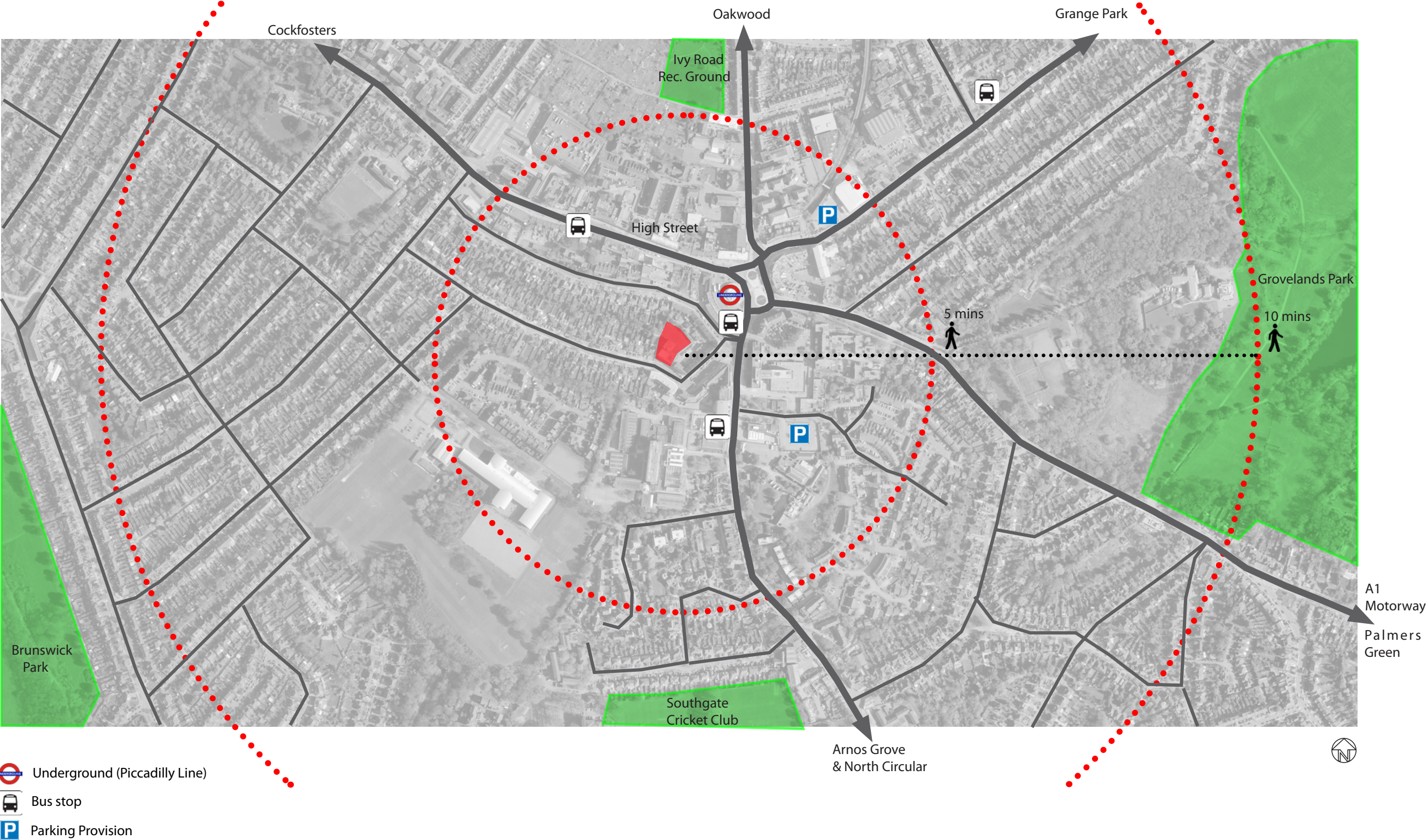
A single storey pitched roof outbuilding is located within the north-eastern corner of the car park and is in a poor state of repair.

The site boundary to the north and east, formed by a low brick wall looks directly on to the back of 3 storey buildings; service access for commercial properties at ground floor and residential properties at first and second. The opposite site of the wall along this boundary is also used to store refuse bins for the commercial units and as parking.

The boundary to the west is bordered by the gardens belonging to the 2 storey semi-detached properties that extend along the length of Burleigh Gardens. There are a number of single storey outbuildings and trees positioned immediately up to the boundary line which is defined by a combination of brick wall and timber fencing.



2.4 Site Accessibility



2.5 Site Photographs

On approach along Burleigh Gardens the site marks a gateway to Southgate’s commercial quarter which continues around the corner onto Ashfield Parade. Ground floor accommodation becomes predominantly A1 and A3 use as the road snakes its way to the high street.

The site also defines a change of scale from the two storey buildings fronting Burleigh Gardens becoming increasing taller and wider. Facing materials also change from predominantly light render to red/brown brick.

The photographs, right, illustrate the character of the area immediately surrounding the site.



1. South East view along Burleigh Gardens



2. West view along Burleigh Gardens



3. East view along Burleigh Gardens



4. West View of opposite neighbour



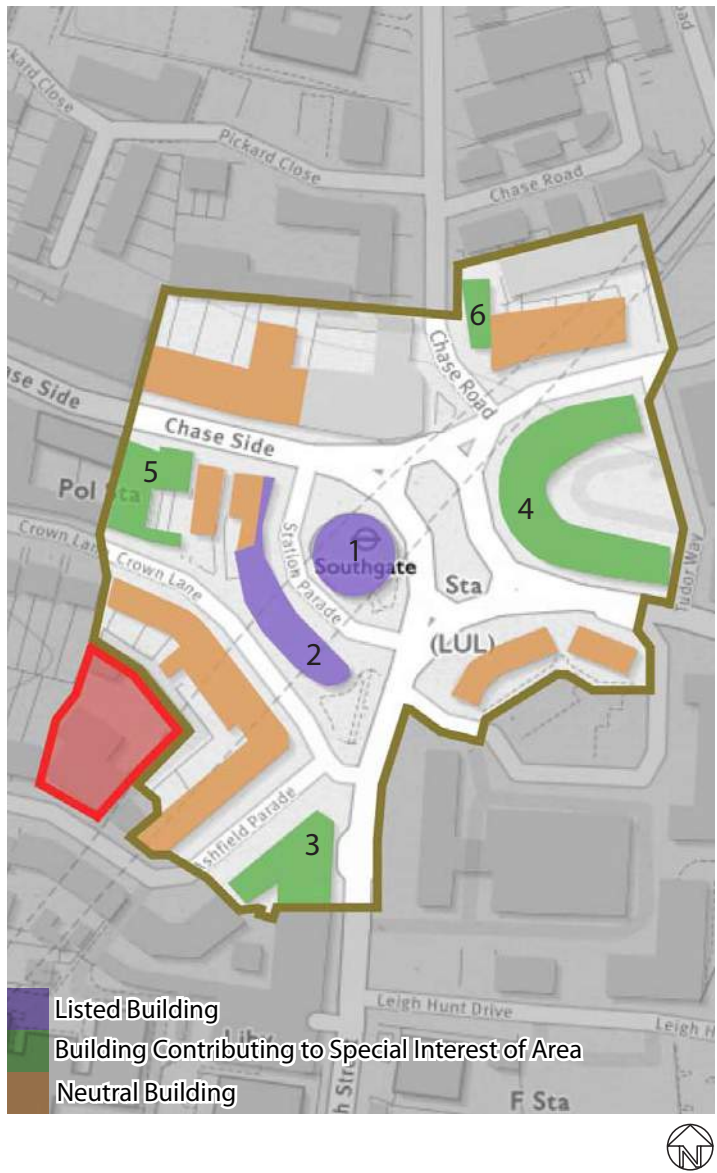
5. West view towards Ashfield Parade



6. North view along Ashfield Parade

2.6 Surrounding Context

The site is located at the border of the Southgate Circus Conservation Area. This town centre location is notable for a number of buildings of historical importance which define the character of the area. These include the Grade II listed Southgate Underground station and surrounding shopping parade which was constructed in the 1930's. Other buildings contributing to the special interest of the area are the three storey terraced buildings along Ashfield parade and pre 19th century buildings along Chase side such as the Southgate Club and the White Hart Public House.



1. Southgate Underground Station from East



4. The Broadway from South West



2. Station Parade from East



5. Southgate Club and Chase Side shops from North



3. Ashfield Parade from North

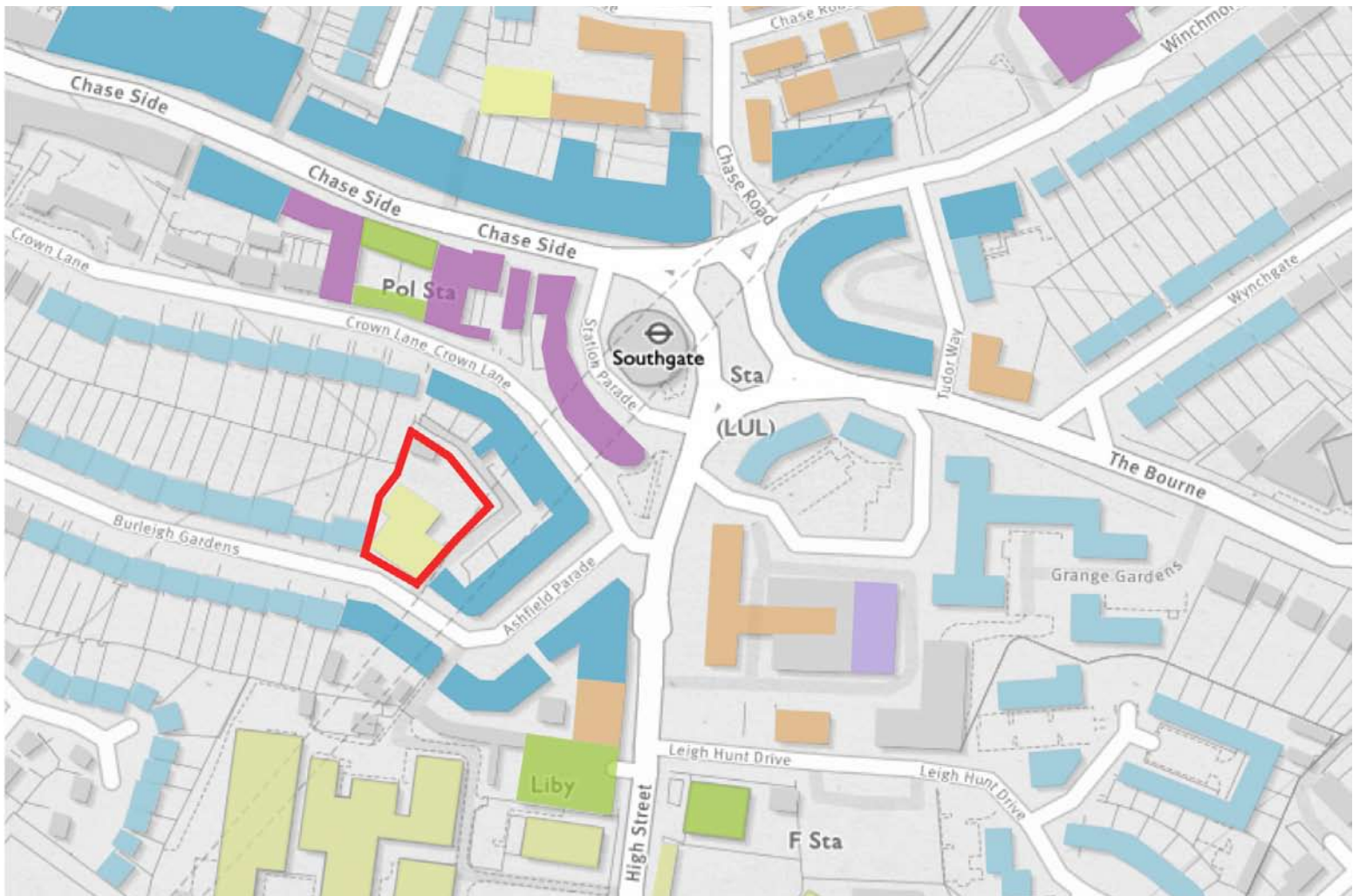


6. The White Hart from West

2.7 Surrounding Typologies

To the west of the site is predominantly residential comprising of a series of semi-detached houses fronting Burleigh Gardens, spreading further west and south. The site marks the start of the commercial district to the east and north which is heavily populated by ground floor retail and food/drink accommodation. Further south are several educational facilities including Southgate College and Ashmole School. Directly east are the mixed use terrace buildings of Ashfield Parade. Beyond the high street is a large office building 'The Grange' and directly behind this construction work has begun on a five storey hotel.

- Residential
- Residential with Ground Floor Commercial
- Educational
- Office
- Commercial
- Public Building



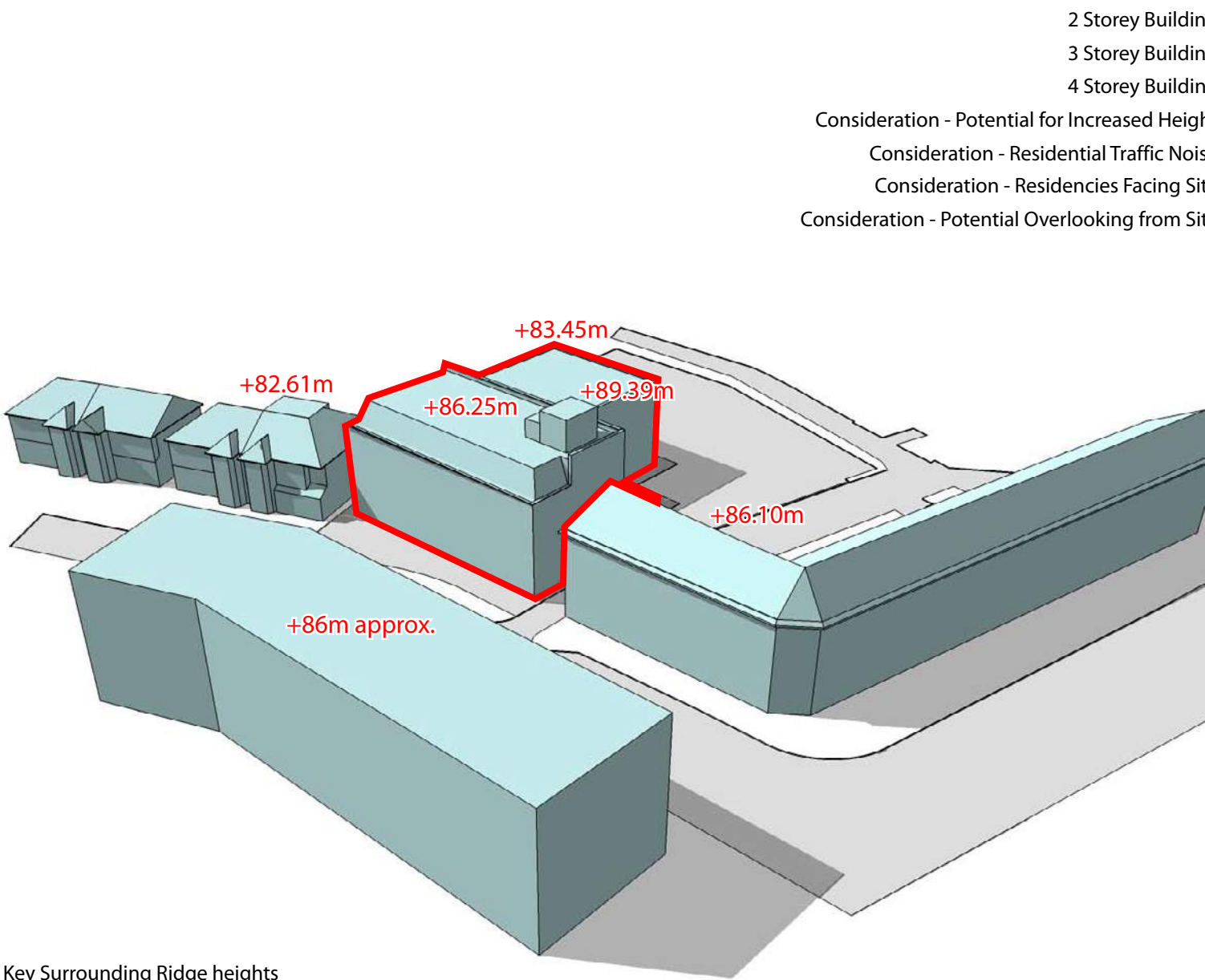
Surrounding Building Typologies



Front Elevation along Burleigh Gardens

2.8 Site Constraints and Opportunities

The diagram opposite illustrates an awareness of the design constraints on the site such as the current overlooking of residential properties to the west. The proposals will seek to address these issues as well as exploring other opportunities which include the addition of a roof extension at the rear that is concealed from views from Burleigh Gardens.

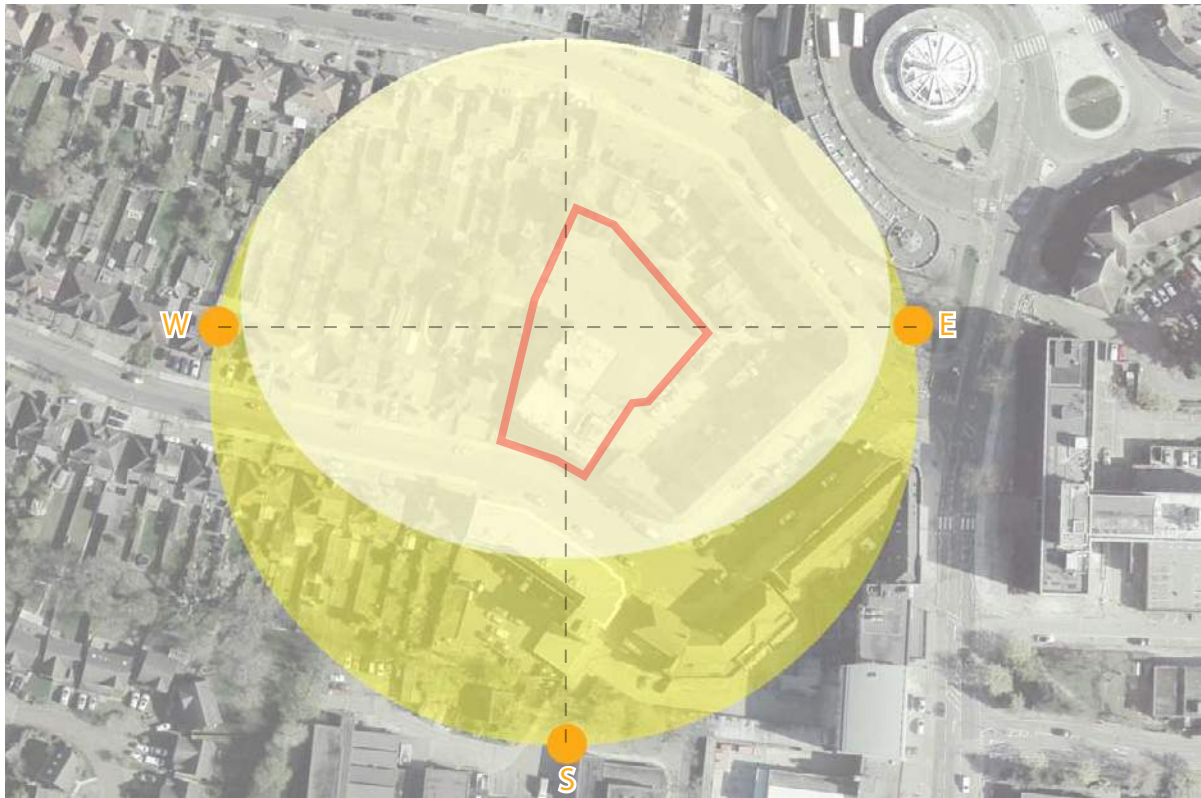


Key Surrounding Ridge heights

- 2 Storey Building
- 3 Storey Building
- 4 Storey Building
- Consideration - Potential for Increased Height
- Consideration - Residential Traffic Noise
- Consideration - Residences Facing Site
- Consideration - Potential Overlooking from Site



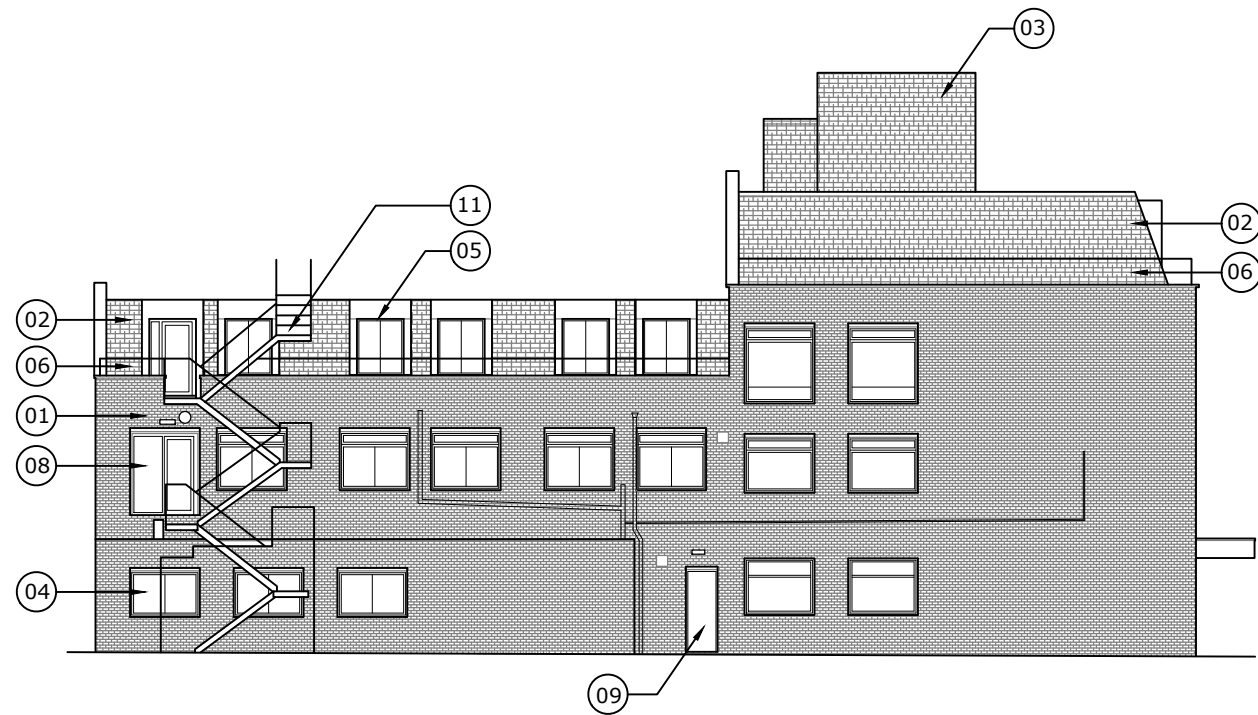
Site Considerations



Sun Tracking Diagram



Rear Elevation



Side Elevation (North West)

- Material Key:
- 01 - Red/Brown Brick

02 - Slate roof covering

03 - Slate clad plant housing

04 - Single glazed metal framed windows

05 - Lead clad dormer window

06 - Metal safety railing

07 - Grey metal canopy & stanchions

08 - Metal framed external doors

09 - Timber framed external doors

10 - Building Signage

11 - Metal external escape stair

The contractor is to check all dimensions on site and report any discrepancies to the architect
All rights described in chapter IV of the copyright, designs and patents act 1988 have been generally asserted

Notes

P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

3rd Floor
7-15 Rosebery Avenue
London EC1R 4SP

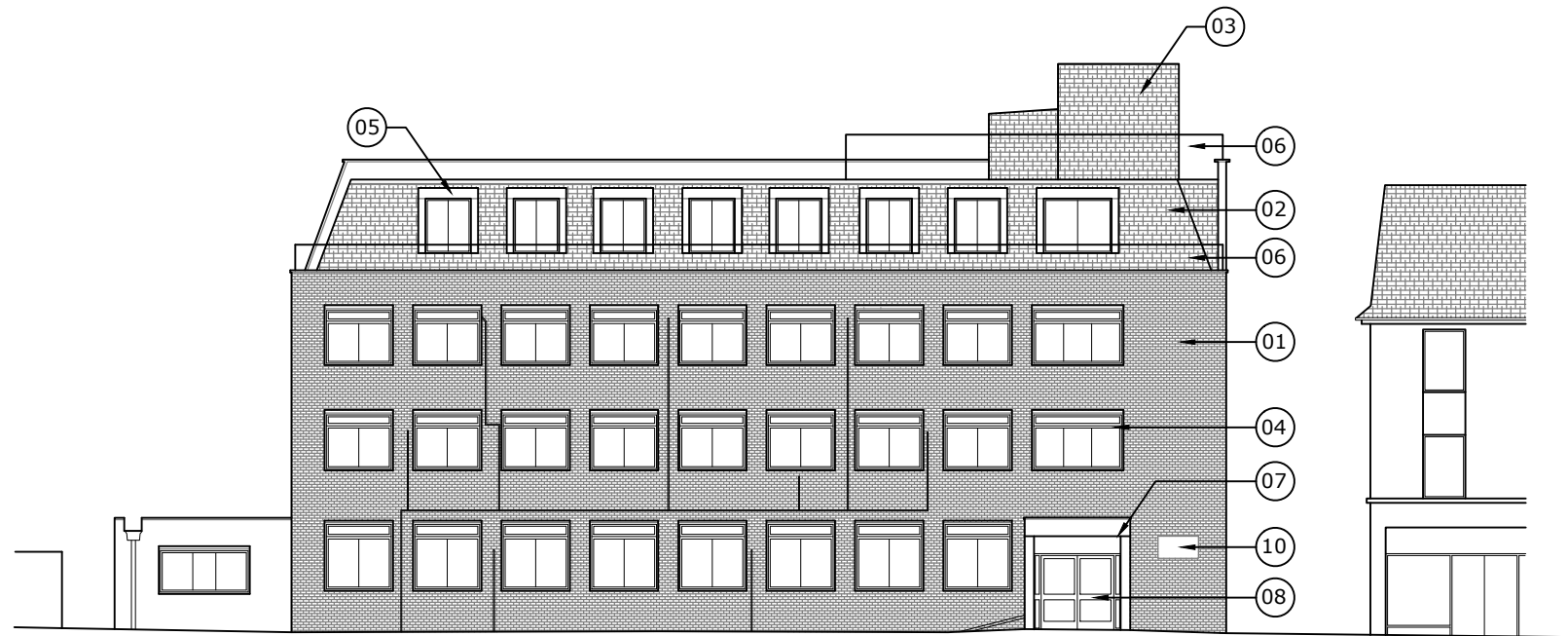
Telephone +44(0)2038746707
Website www.jeffersonsheard.com



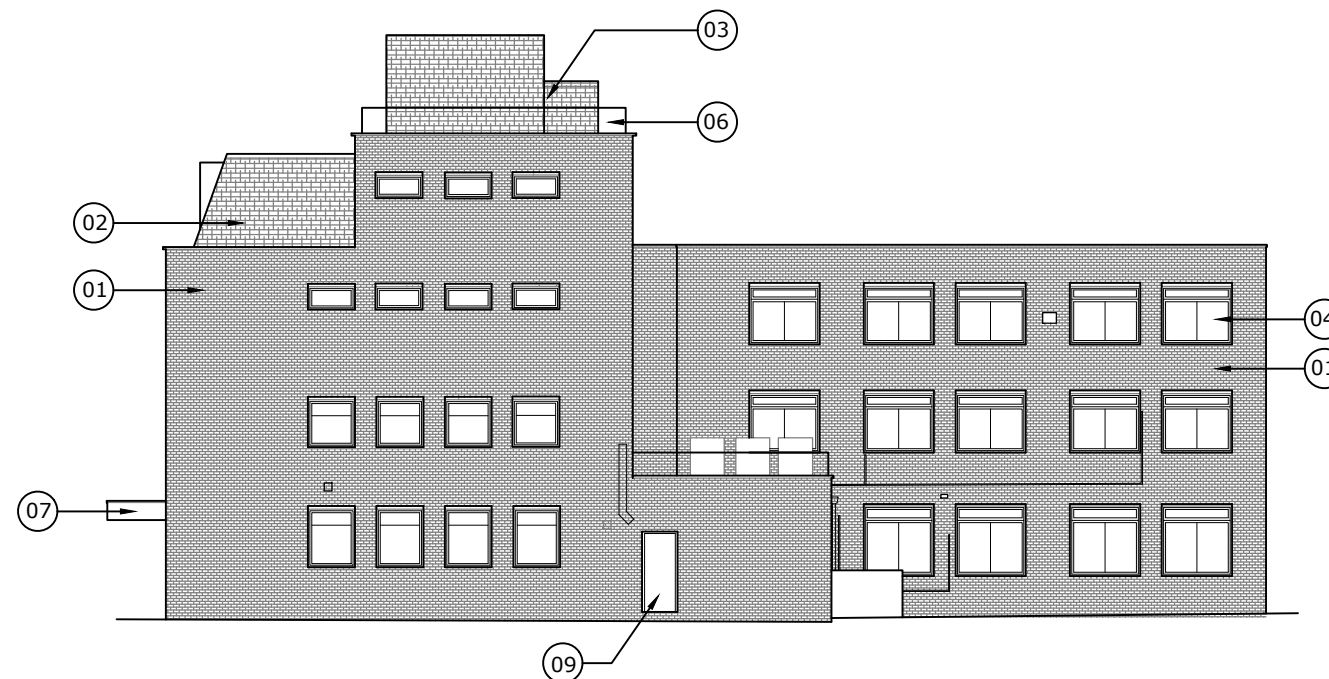
Project
93 Burleigh Gardens

Drawing
Elevations as Existing

Scale 1:200@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-00-DR-A-04002		Rev. P1



Front Elevation (South West)



Side Elevation (South East)

Material Key:

- | | |
|---|-------------------------------------|
| 01 - Red/Brown Brick | 07 - Grey metal canopy & stanchions |
| 02 - Slate roof covering | 08 - Metal framed external doors |
| 03 - Slate clad plant housing | 09 - Timber framed external doors |
| 04 - Single glazed metal framed windows | 10 - Building Signage |
| 05 - Lead clad dormer window | 11 - Metal external escape stair |
| 06 - Metal safety railing | |

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Rev	Description	Date	By	Chk

3rd Floor
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London EC1R 4SP

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Website www.jeffersonsheard.com



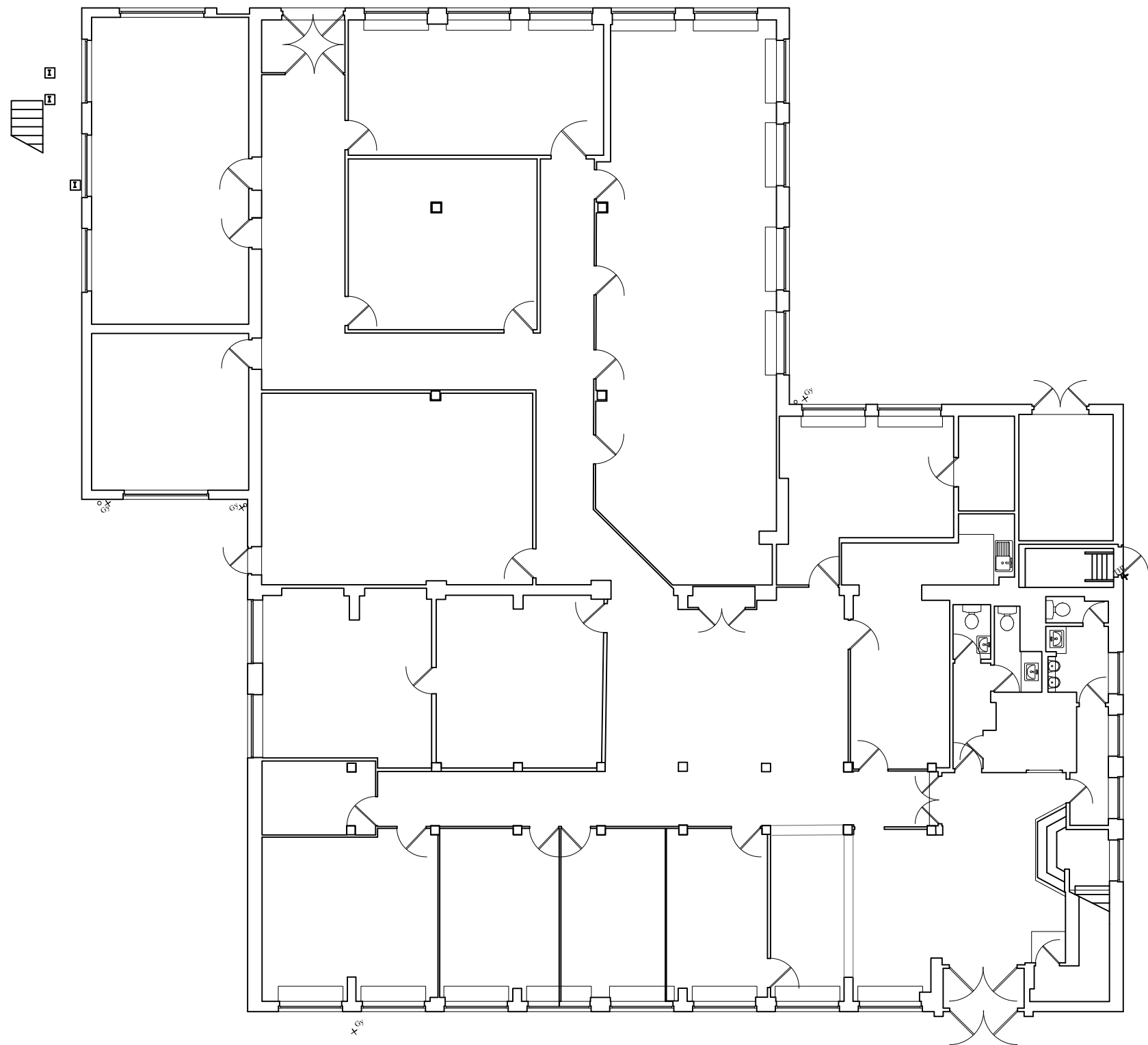
Project

93 Burleigh Gardens

Drawing

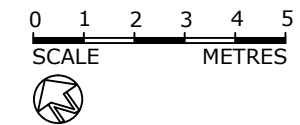
Elevations as Existing

Scale 1:200@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-00-DR-A-04001		Rev. P1



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P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

3rd Floor
7-15 Rosebery Avenue
London EC1R 4SP
Telephone +44(0)2038746707
Website www.jeffersonsheard.com

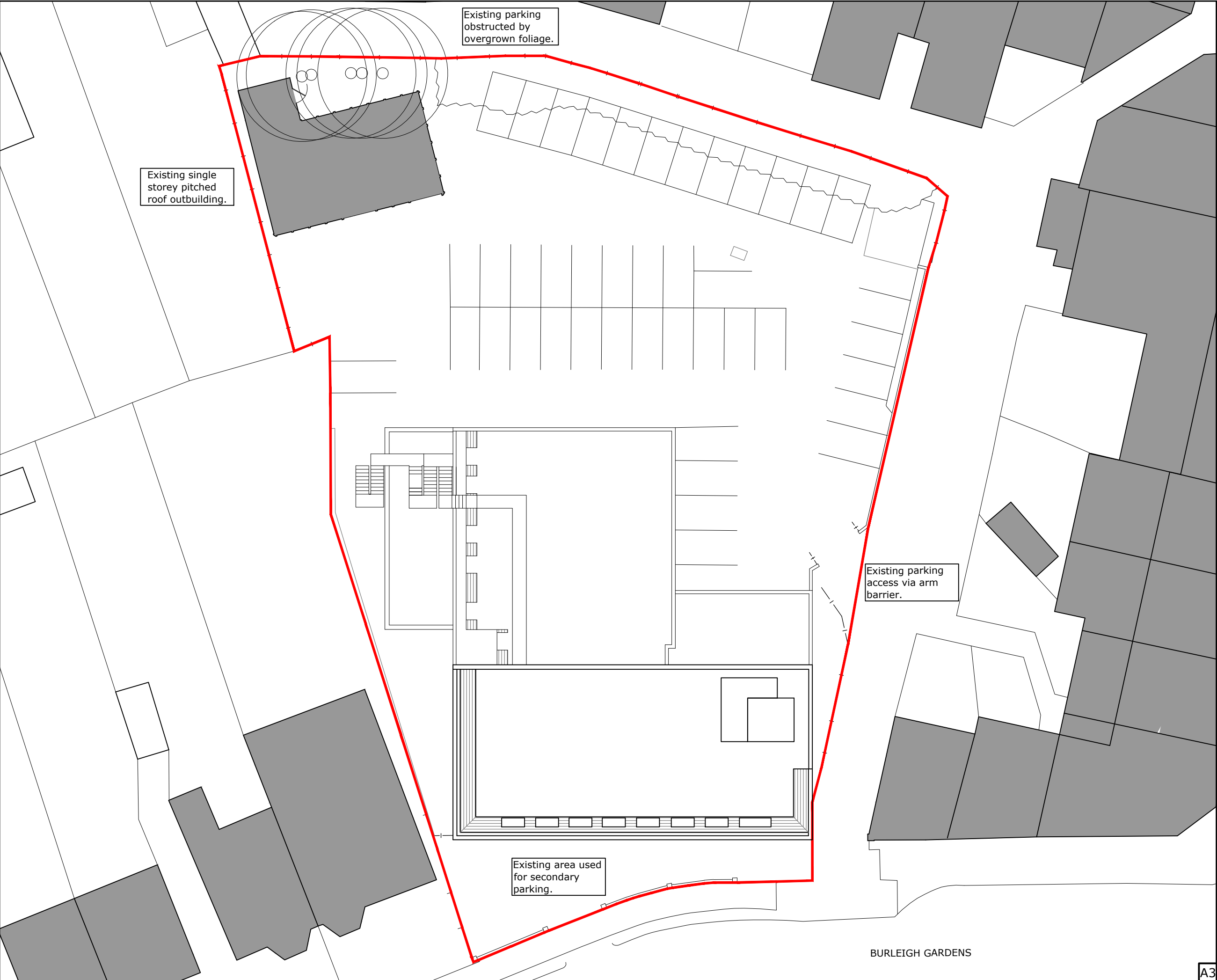


Project
93 Burleigh Gardens

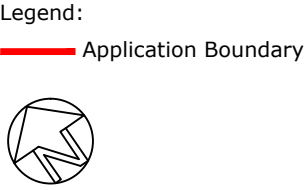
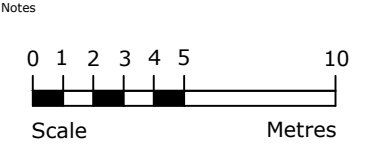
Drawing
Ground Floor as Existing

Scale 1:150@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-00-DR-A-02001		Rev. P1

A3



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P1	Initial Issue	26/04/17	JR	JSA
Rev	Description	Date	By	Chk

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Jefferson
Sheard
Architects

Project
93 Burleigh Gardens

Drawing
Existing Site Plan

Scale 1:250@A3	Drawn JR	Date 26.04.17
Drawing Purpose PLANNING SUBMISSION	Status S5	
Ref. 0883-JSA-XX-00-DR-A-01001	Rev. P1	

93 Burleigh Gardens - Entrance Canopy Study



Existing entrance canopy

The existing top heavy entrance canopy is a bulky and unattractive addition to the building facade.

The proposed entrance doors are to be set back from the building facade to create a distinctive, sheltered entrance space that introduces a sense of depth to a currently lifeless facade.

In addition, the slim extruded metal reveal around the glazed opening will be extended further to provide sheltered standing space, as well as serving to denote the main building entrance.



Proposed visual on approach along Burleigh Gardens (south east)



Proposed visual on approach along Burleigh Gardens (north west)

93 Burleigh Gardens - Fire Escape Study



Existing external escape stair photographs



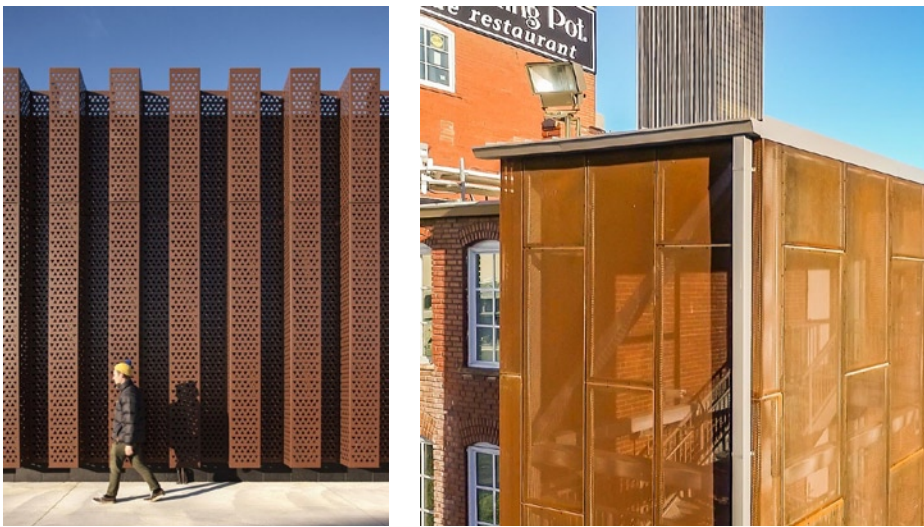
93 Burleigh Gardens - Fire Escape Study



Proposed Rear Elevation (North East)



Proposed Side Elevation (North West)

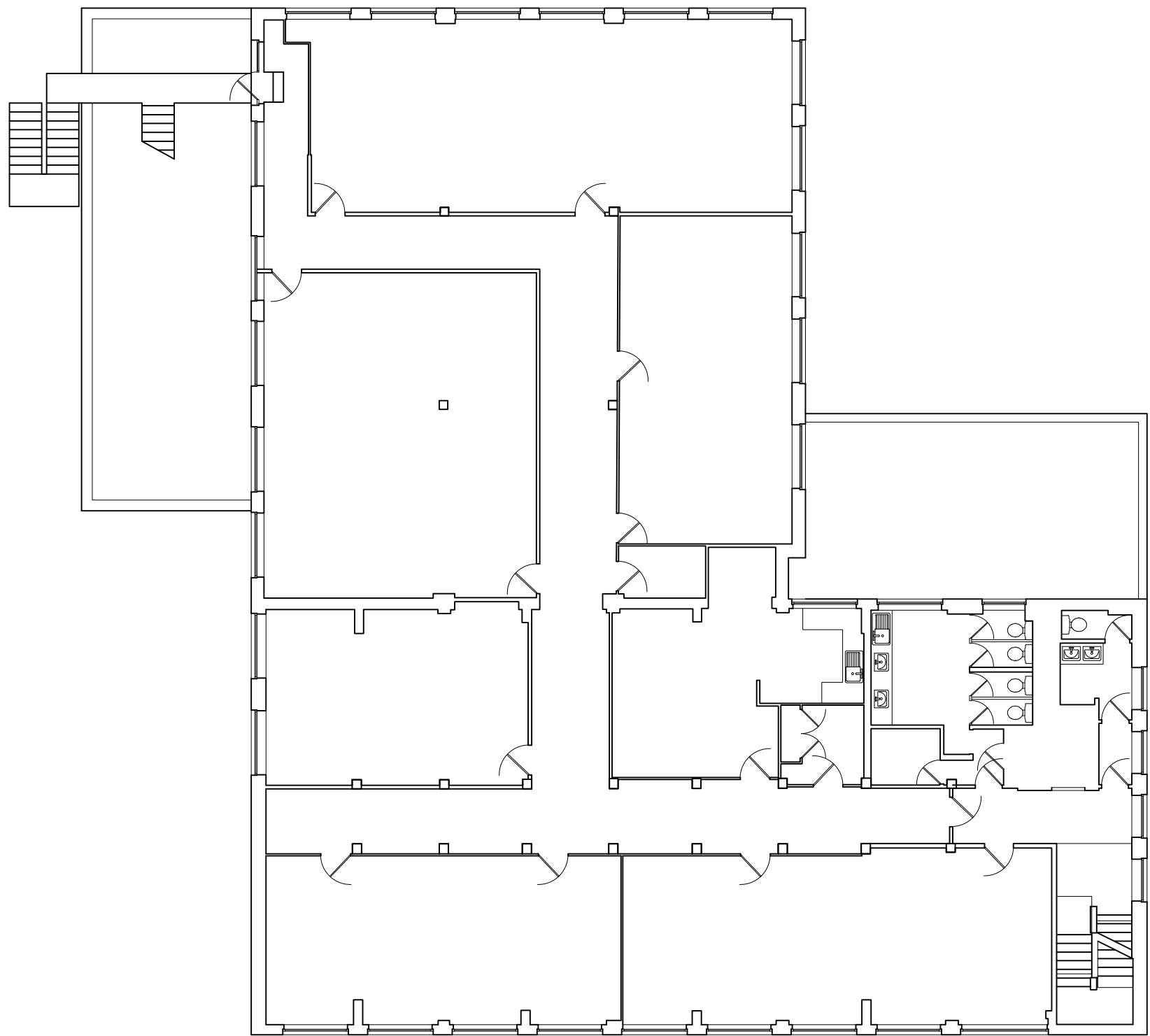


Examples of perforated bronze facade panelling



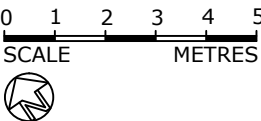
Proposed visual from West

Composite bronze panelling to match elements of the main building facade is proposed to enclose the upper levels of the reconfigured external fire escape. Strategic perforated sections will supply natural light to the stair whilst still creating privacy and restricting overlooking. The section of stair from ground to first will be hidden by the existig boundary wall to properties along Burleigh Gardens.



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Notes



P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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Project
93 Burleigh Gardens

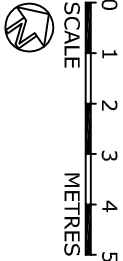
Drawing
First Floor as Existing

Scale 1:150@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-01-DR-A-02001		Rev. P1

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Notes



P2	External Fire Escape	11/09/17	JR	JSA
P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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Website www.jeffersonsheard.com

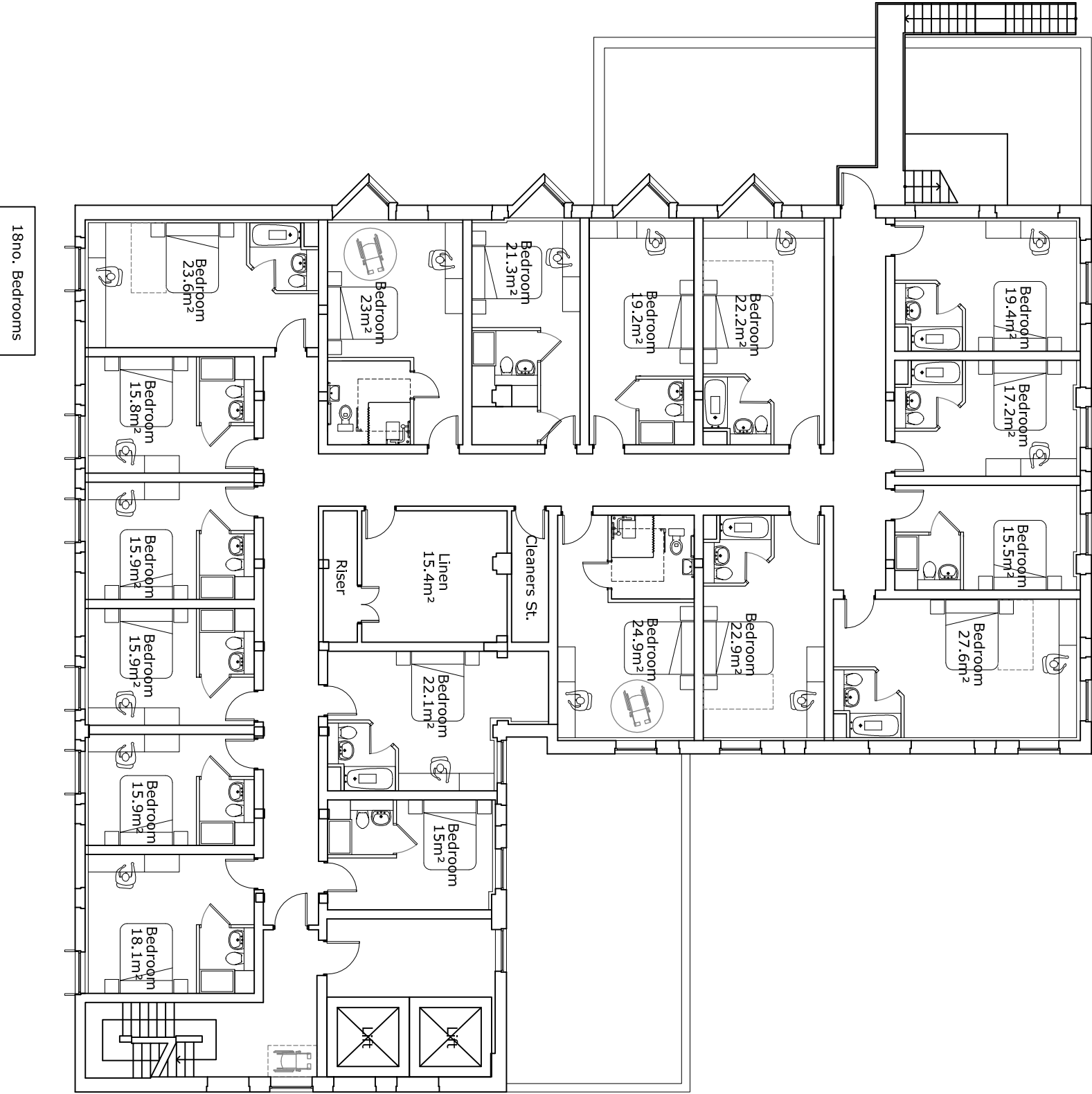


Project:
93 Burrelgh Gardens

Drawing

First Floor as Proposed

Scale	Drawn	Date
1:150@A3	JR	27.04.16
Drawing Purpose	Status	
PLANNING SUBMISSION	S5	
Ref.	Rev.	
0883-JSA-XX-01-DR-A-02002	P2	



18no. Bedrooms

A3



Front Elevation (South West)

Material Key:

- 01 - New brick slip cladding system

02 - New off-white through render

03 - New metal facade panel

04 - New rainscreen cladding system

05 - New roof slate

06 - New extruded metal window reveal

07 - New metal clad angled window box

08 - New metal lining to match facade panels

09 - New aluminium framed double glazing
- 10 - New aluminium framed external doors

11 - New timber framed external door

12 - New metal external escape stair

13 - New concrete coping

14 - New lead cladding to dormer window

15 - New Lead clad angled dormer window

16 - New lift plant clad in slate

17 - New metal signage panel

18 - New angled window box in rainscreen cladding

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Notes

P1	Initial Issue	24.04.17	JR	JSA
Rev	Description	Date	By	Chk

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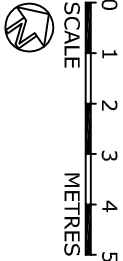
Project
93 Burleigh Gardens

Drawing
Proposed Elevation

Scale 1:100@A3	Drawn JR	Date 24.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-XX-DR-A-04003	Rev. P1	

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Notes



P2	External Fire Escape	11/09/17	JR	JSA
P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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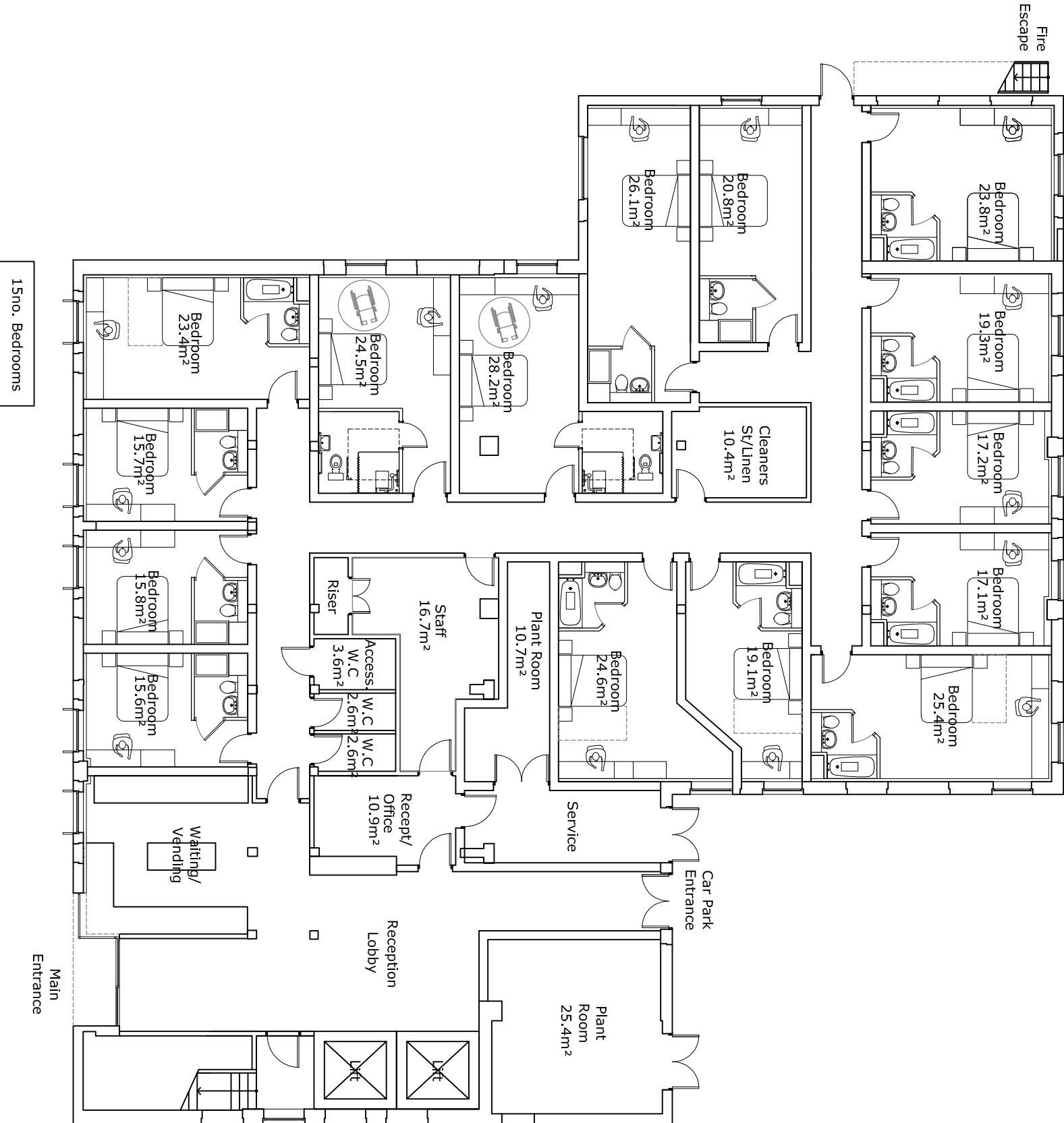


Project:
93 Burreigh Gardens

Drawing

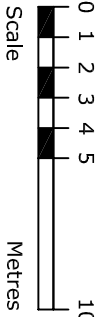
Ground Floor as Proposed

Scale	Drawn	Date
1:150@A3	JR	27.04.17
Drawing Purpose	Status	
PLANNING SUBMISSION	S5	
Ref.	Rev.	
0883-JSA-XX-00-DR-A-02002	P2	



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Notes



Key

- Existing tree maintained
- Existing planting trimmed and maintained
- New shrubbery
- New metal planter box - tree
- New metal planter box - shrub
- New brick planting wall - shrub
- New area of grass
- New concrete pavers
- New feature block paving
- New feature cobbled paving
- New permeable paving to form part of drainage scheme

P1	Initial Issue	08/09/17	JR	-
Rev	Description	Date	By	CHK

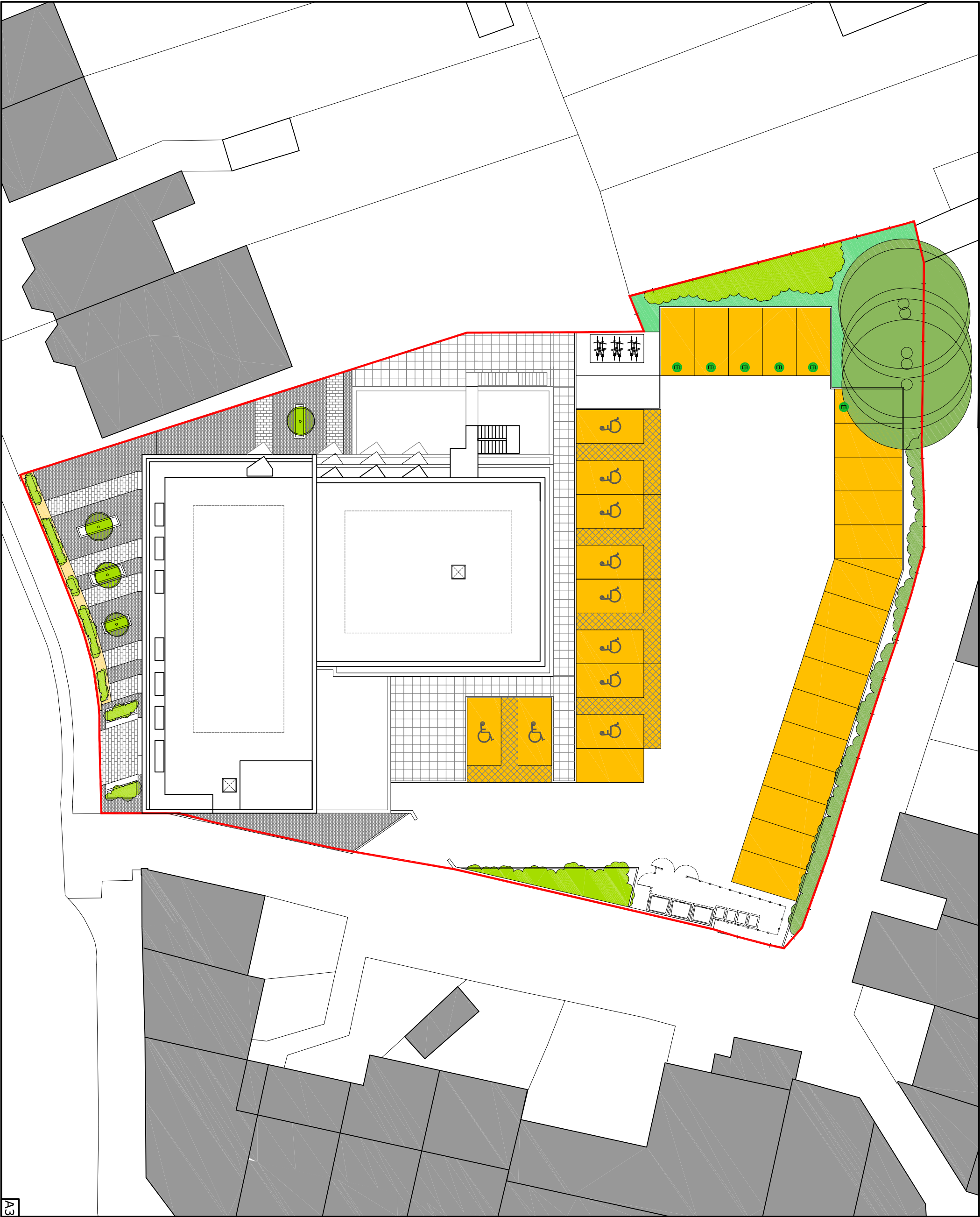
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Project
93 Burleigh Gardens

Drawing
Indicative
Landscaping Plan

Scale	1:250@A3	Drawn	JR	Date	08.09.17
Drawing Purpose	PRELIMINARY	Status	SO	Rev.	P1
Ref.	0883-JSA-XX-XX-DR-A-90201				



A3

Dashed line indicates previous roof plant outline.



Rear Elevation (North East)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |

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Notes

P1	Initial Issue	24.04.17	JR	JSA
Rev	Description	Date	By	Chk

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Project
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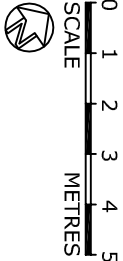
Drawing
Proposed Elevations

Scale 1:100@A3	Drawn JR	Date 24.04.17
Drawing Purpose PLANNING SUBMISSION	Status S5	
Ref. 0883-JSA-XX-XX-DR-A-04005	Rev. P1	

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Notes



P2	Green Roof/Fescape	08/09/17	JR	JSA
P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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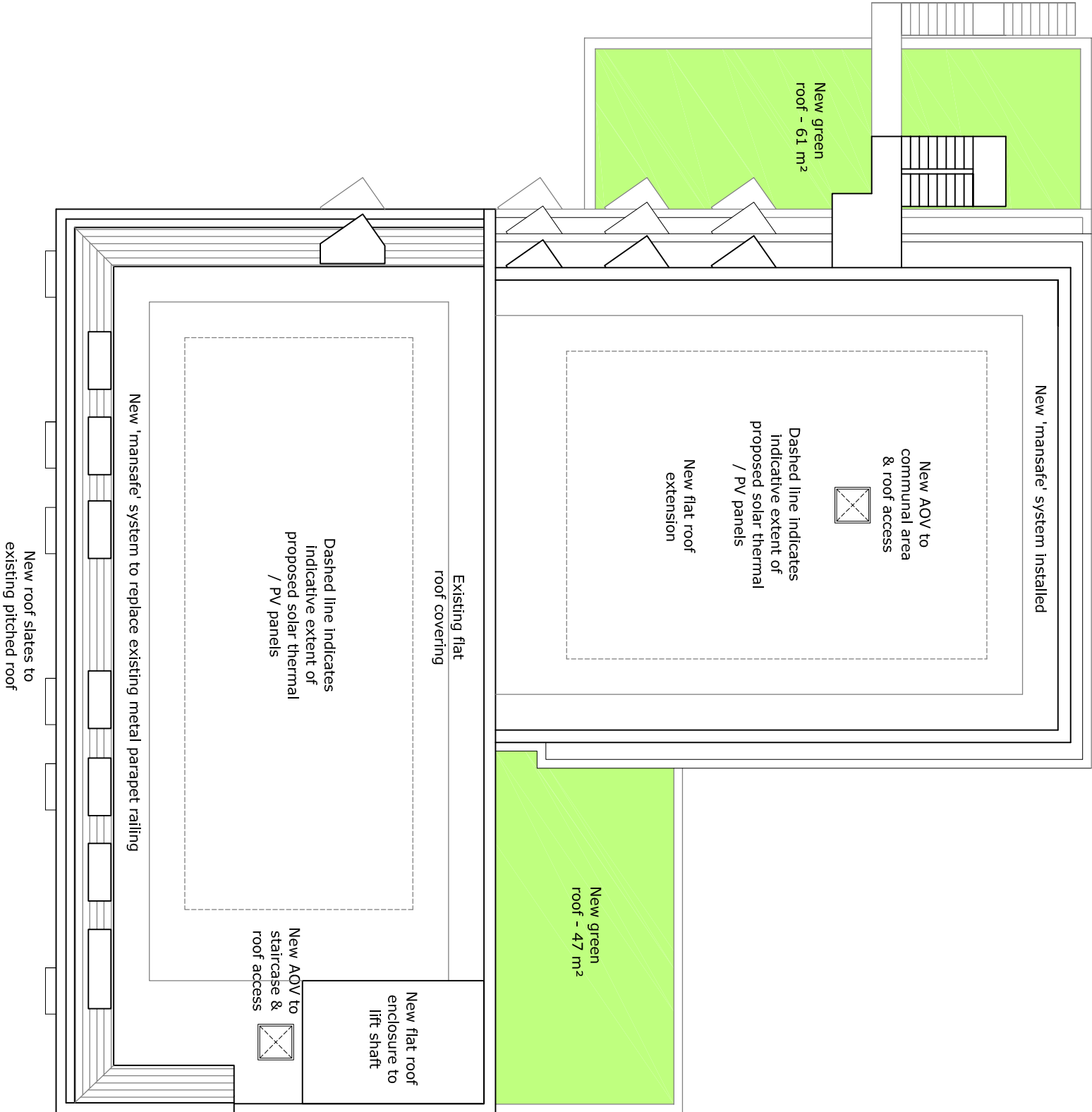


Project:
93 Burrelgh Gardens

Drawing

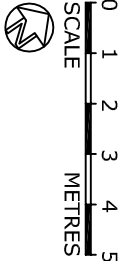
Proposed Roof Plan

Scale	Drawn	Date
1:150@A3	JR	08.09.17
Drawing Purpose	Status	
PLANNING SUBMISSION	S5	
Ref.	Rev.	
0883-JSA-XX-XX-DR-A-27001	P2	



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Notes



P2	External Fire Escape	11/09/17	JR	JSA
P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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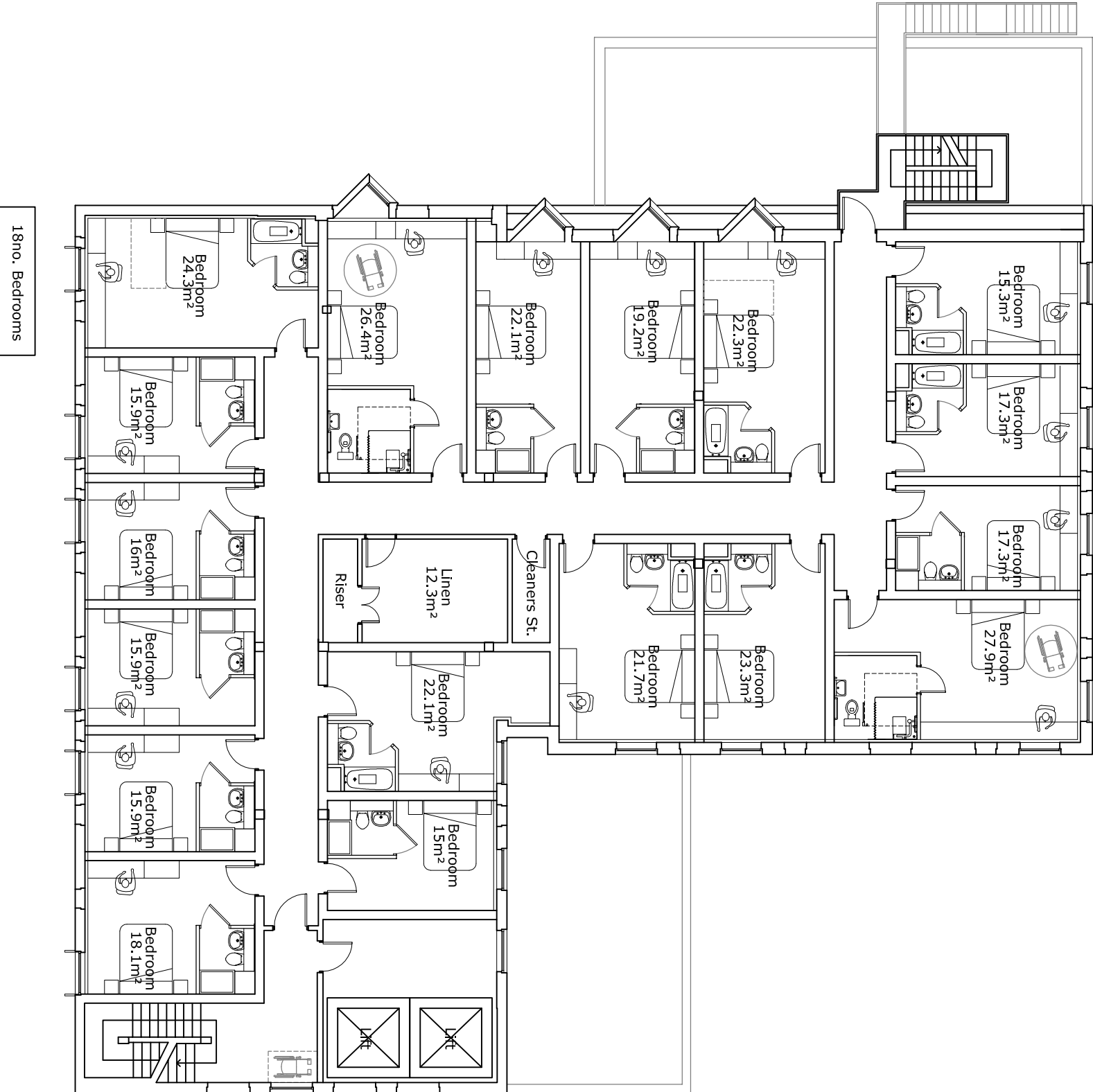


Project:
93 Burreigh Gardens

Drawing

Second Floor as Proposed

Scale	1:150@A3	Drawn	JR	Date	27.04.17
Drawing Purpose	PLANNING SUBMISSION	Status	S5	Rev.	P2
Ref.	0883-JSA-XX-02-DR-A-02002				



18no. Bedrooms



Side Elevation (North West)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |

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Notes

P1	Initial Issue	24.04.17	JR	JSA
Rev	Description	Date	By	Chk

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Jefferson
Sheard
Architects

Project

93 Burleigh Gardens

Drawing

Proposed Elevations

Scale 1:100@A3	Drawn JR	Date 24.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-XX-DR-A-04006		Rev. P1

A3

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Notes



Side Elevation (South East)

Material Key:

- | | |
|--|---|
| 01 - New brick slip cladding system | 10 - New aluminium framed external doors |
| 02 - New off-white through render | 11 - New timber framed external door |
| 03 - New metal facade panel | 12 - New metal external escape stair |
| 04 - New rainscreen cladding system | 13 - New concrete coping |
| 05 - New roof slate | 14 - New lead cladding to dormer window |
| 06 - New extruded metal window reveal | 15 - New Lead clad angled dormer window |
| 07 - New metal clad angled window box | 16 - New lift plant clad in slate |
| 08 - New metal lining to match facade panels | 17 - New metal signage panel |
| 09 - New aluminium framed double glazing | 18 - New angled window box in rainscreen cladding |

P1	Initial Issue	24.04.17	JR	JSA
Rev	Description	Date	By	Chk

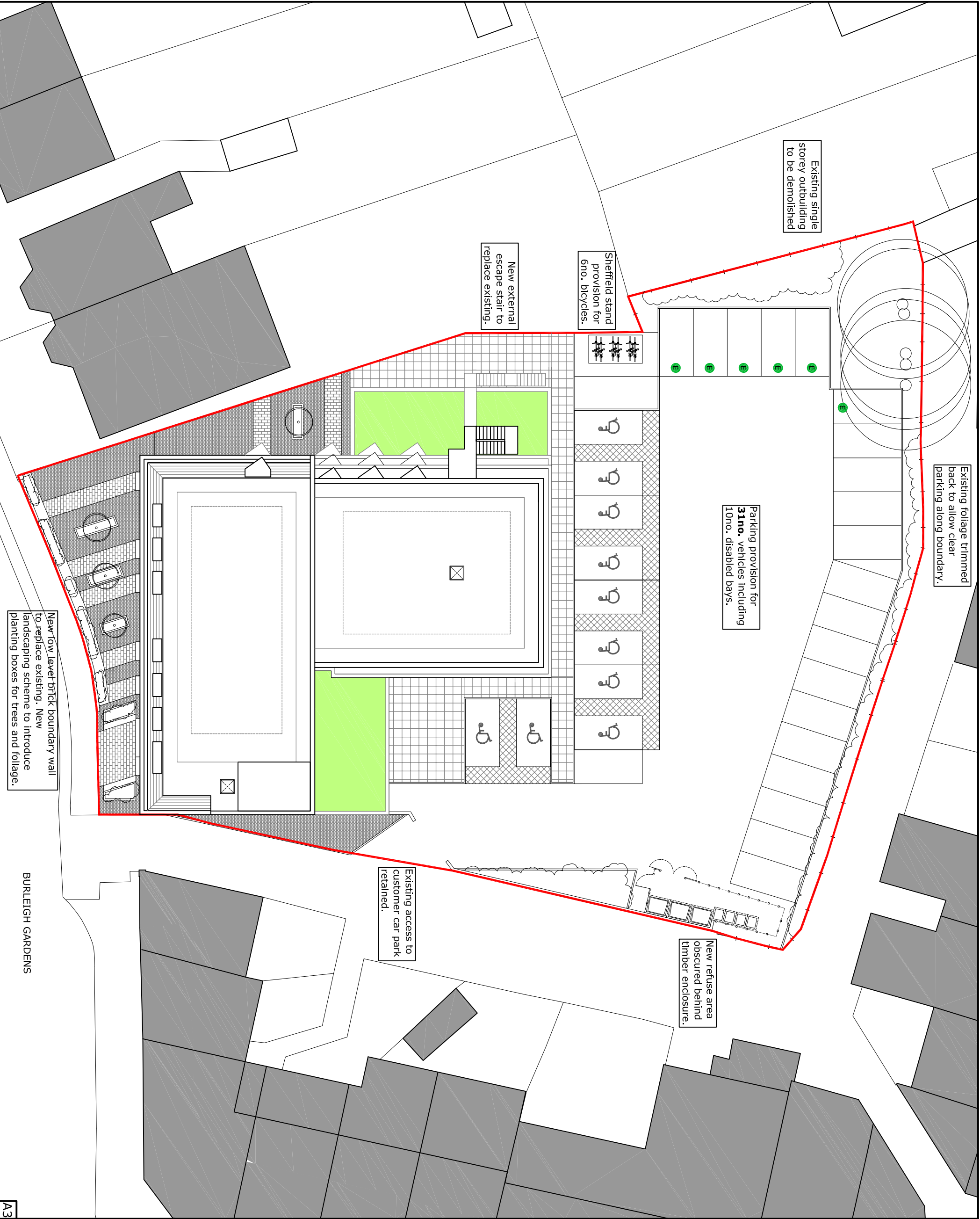
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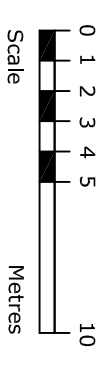
Drawing
Proposed Elevations

Scale 1:100@A3	Drawn JR	Date 24.04.17
Drawing Purpose PLANNING SUBMISSION	Status S5	
Ref. 0883-JSA-XX-XX-DR-A-04004	Rev. P1	



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Notes



Legend:

- Application Boundary
- Electric Vehicle Charging (including 3no. spaces for future provision)



P3	External Fire Escape	11/09/17	JR	JD
P2	Parking Amendments	31/07/17	JR	JD
P1	Initial Issue	26/04/17	JR	JSA
Rev	Description	Date	By	Chk

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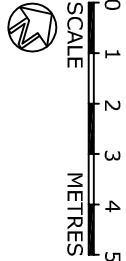
Drawing

Proposed Site Plan

Scale 1:250@A3	Drawn JR	Date 31.07.17
Drawing Purpose PRELIMINARY	Status S0	Rev. P3
Ref. 0883-JSA-XX-00-DR-A-01002		

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Notes



P2	External Fire Escape	11.08.17	JR	JSA
P1	Initial Issue	27.04.17	JR	JSA
Rev	Description	Date	By	Chk

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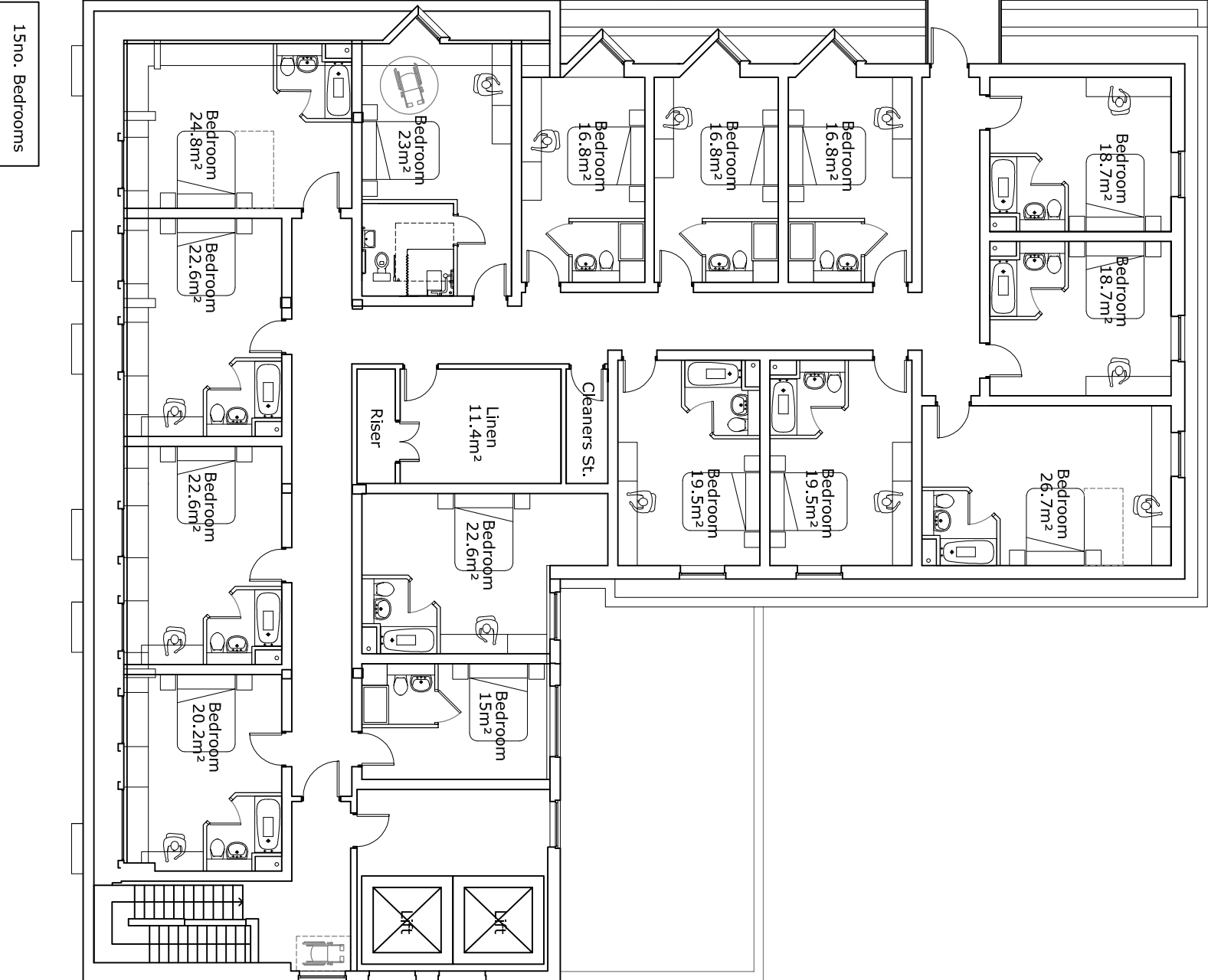


Project:
93 Burreigh Gardens

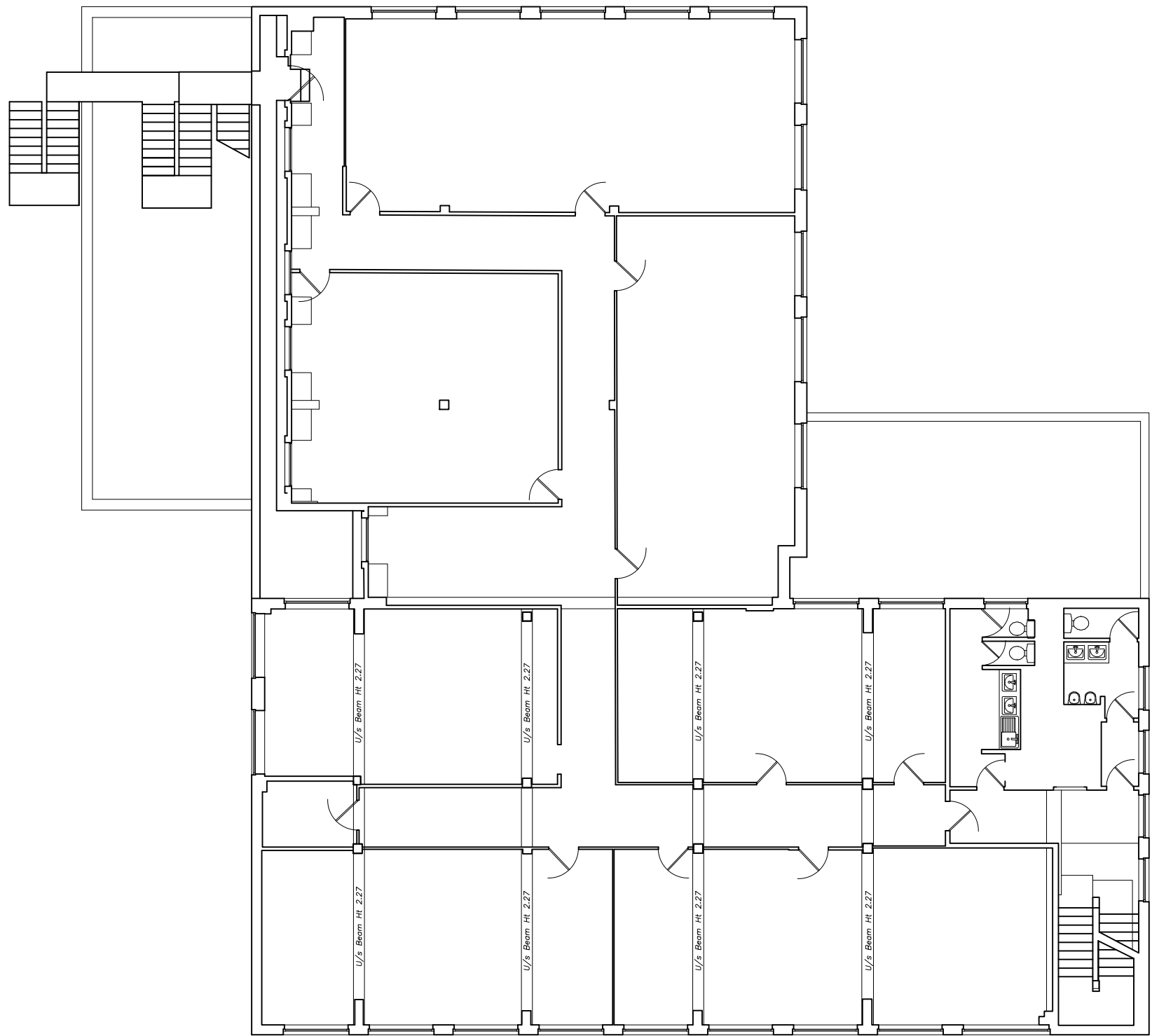
Drawing

Third Floor as Proposed

Scale	Drawn	Date
1:150@A3	JR	27.04.17
Drawing Purpose	Status	
PLANNING SUBMISSION	S5	
Ref.	Rev.	
0883-JSA-XX-03-DR-A-02002	P1	

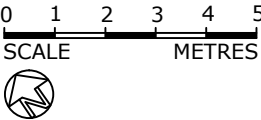


A3



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Notes



P1	Initial Issue	27/04/17	JAR	JSA
Rev	Description	Date	By	Chk

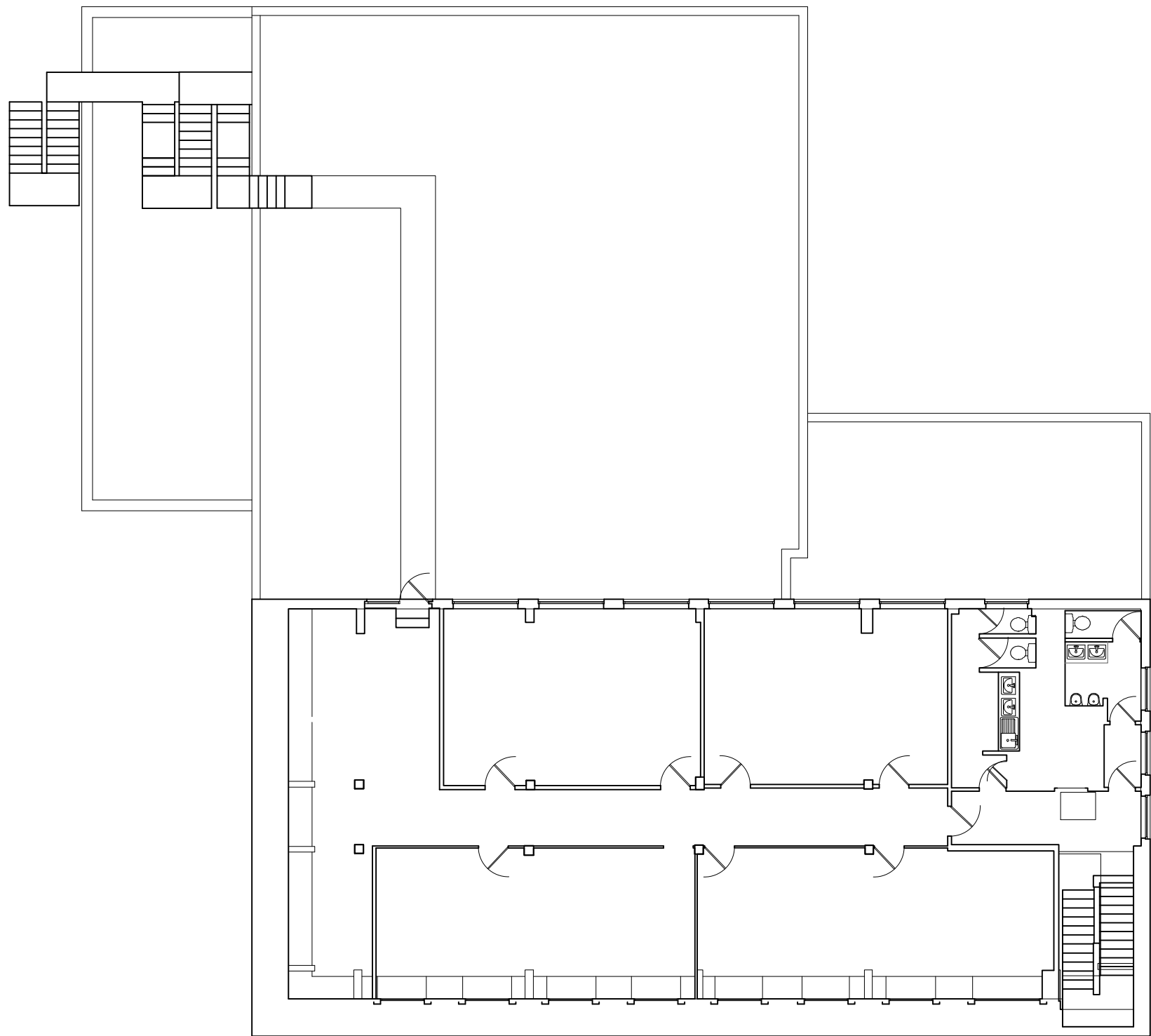
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Drawing
Second Floor as Existing

Scale 1:150@A3	Drawn JR	Date 27.04.17
Drawing Purpose PLANNING SUBMISSION		Status S5
Ref. 0883-JSA-XX-02-DR-A-02001		Rev. P1

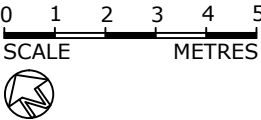


Do not scale from this drawing

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Notes



P1	Initial Issue	27/04/17	JR	JSA
Rev	Description	Date	By	Chk

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Project

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Drawing

Third Floor as Existing

Scale	Drawn	Date
1:150@A3	JR	27.04.17
Drawing Purpose		Status
PLANNING SUBMISSION		S5
Ref.	Rev.	
0883-JSA-XX-03-DR-A-02001	P1	